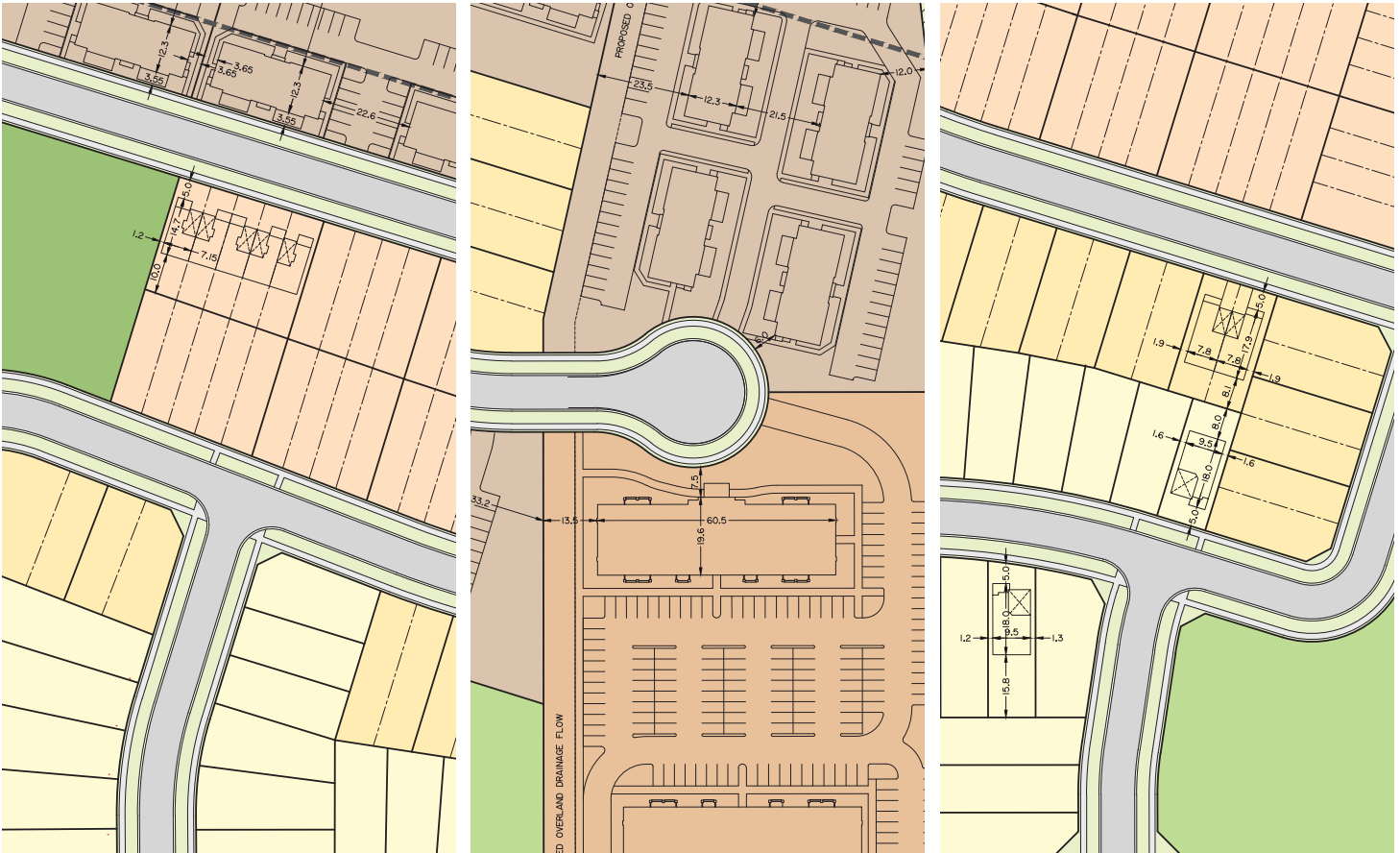


PLANNING REPORT

East Village - Phase 5
JMCD HOLDINGS INC.
460 King Street East
TOWN OF COBOURG



Submitted By:

RFA Planning Consultant Inc.

202-211 Dundas Street East, Belleville, Ontario, K8N 1E2

DECEMBER, 2018



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1.0 INTRODUCTION

1.1 The East Village –Phase 5 Project

Applications for draft approval of a Plan of Subdivision, Official Plan Amendment and Rezoning for 460 King Street East have been filed with the Town of Cobourg Planning Department in December 2018. This Planning Report has been prepared in support of the municipal applications. The owner/applicant is JMCD Holdings Inc. and the builder is Stalwood Homes. Stalwood is currently building in Phases 1-4 of the East Village subdivision, immediately west of the subject lands (Plan 39M-875). In the 2000's, an application was filed to draft approve all of Phases 1-5; the Town at that time approved only Phase 1 through 4. Phase 3 of Plan 875 (East Village) is currently under construction.

1.2 Support Studies and Consultant Team

The applicant met with the Town of Cobourg Development Review Team (DRT) in October 2018 to review the submission requirements for the subdivision. At that time, the Town advised that several technical studies would be required to support the project. In support of the application for draft plan approval, the following technical studies were prepared and have been submitted under separate cover:

- East Village Subdivision Stormwater Management Report, Engage Engineering, December 2018
- Geotechnical Investigation Report, GHD, February 2, 2018
- East Village Phase 5 Functional Servicing Report, Engage Engineering, November 2018
- East Village Phase 5 Downstream Trunk Storm Sewer Capacity Assessment, Engage Engineering, November 2018
- Environmental Noise Analysis Update, Valcoustics Canada Ltd, December 17, 2018
- Traffic Impact Assessment Update, TranPlan Associates, December 2018
- Environmental Impact Study (EIS), Cambium Inc., November 12, 2018.
- Phase One ESA Report, GHD, December 2017
- Phase Two ESA Report, GHD, February 2018

A Stage 1/2 Archaeological Assessment was previously prepared for all of the subdivision lands (including Phases 1-4) by Archaeogix Inc. in 2007 and is already on file with the Town. The property was found to have no archaeological resources and is considered to have been cleared of all archaeological concerns.

2.0 BACKGROUND INFORMATION

2.1 Site Location

The subject property is located in the Town of Cobourg at 460 King Street East. It is described as Part of Lot 11 & 12, Concession A, Former Township of Hamilton. The overall site area is 11.87 hectares and the property is vacant.



Figure 1: Site Location

2.2 Surrounding Land Uses and Site Features

A summary of the existing site features and land uses is as follows:

- The site is at the eastern edge of the urban area of Cobourg.
- To the north is the CN/CP rail corridor.
- To the west, the lands are being actively being developed for Phases 1-4 of the East Village residential subdivision. The builder, Stalwood Homes has been constructing homes in this subdivision (see photos to follow).
- The land uses to the south and west are low and medium density residential.
- To the south-east of the subdivision, the lands are developed or approved for employment and commercial uses.
- There are rural lands immediately to the east.
- The site is relatively flat with slight north to south slopes.
- The site has been previously cleared and graded.
- There are no watercourses or wetlands on the property, however there is a drainage ditch that conveys surface water from the north down to King Street East.
- The site is predominantly disturbed and sparsely vegetated with buckthorn, scattered trees (Manitoba Maple, Willow, Locust and Aspen), shrubs, grasses and common plants that have been established as the result of the historical disturbance to the site.



View west from Rollings Street of existing townhouse block.



View east from Rollings Street of existing semi-detached dwelling units.



View east from Rollings Street of a single-detached dwelling unit.



Single detached dwelling unit on Rollings Street.



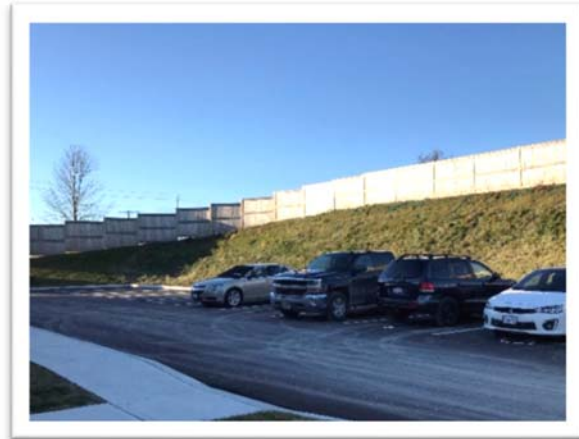
View of the single detached dwellings on Rollings Street.



View of front elevation of 10-Plex Dwelling.



View of side elevation of 10-Plex Dwelling.



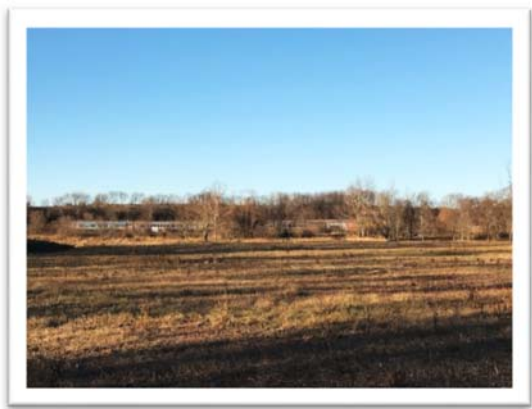
Parking, berm and fencing along north at CP/CN Rail line for 10-Plex Units



Subject property, looking north from King Street East.



Subject property, looking north east from King Street East.



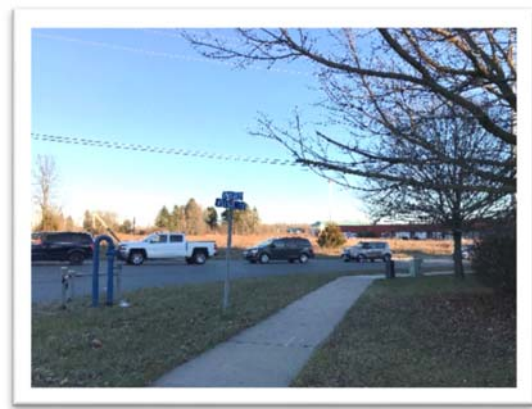
Subject property, with CP/CN Rail Train at the north limit of the site.



Western limits of subject property.



Looking north at subject property from the south side of the Maplewood Blvd and King Street East intersection.



Looking north at the eastern limit of the site from the south side of the Willmott Street and King Street East intersection.



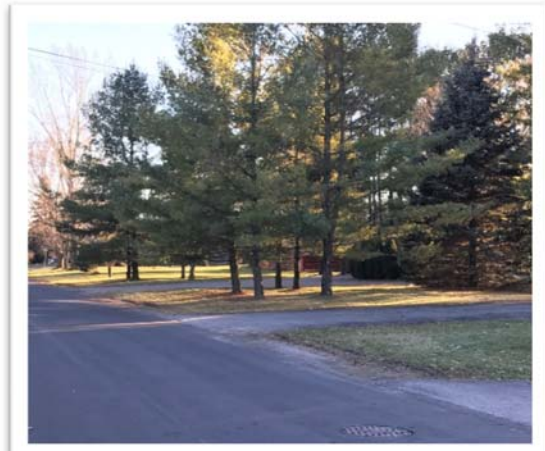
East view of south side of King Street East
across from the subdivision near Willmott
Street.



West view of south side of King Street East
across from the subdivision near Willmott
Street.



Fitzhugh Lane south of King Street East



Maplewood Blvd. south of King Street East

3. PROPOSED DRAFT PLAN

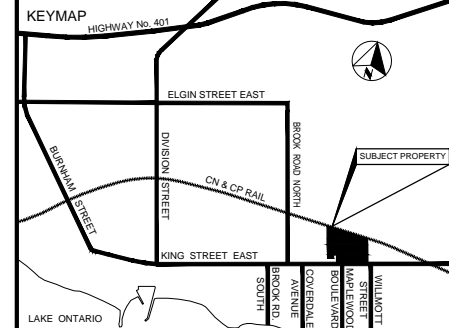
3.1 Subdivision Layout

The Draft Plan of Subdivision drawing was prepared by RFA Planning Consultant Inc. dated December 14, 2018. The draft plan of subdivision is shown on **Figure 2**. It is proposed to register a plan of subdivision to create 61 blocks lots for 333 dwelling units. Blocks are also proposed for parkland dedication, stormwater management, and road widening. New internal road allowances would be created that extend Drewery Road, Hayward Street and Maplewood Boulevard. The land use summary of the subdivision is as follows:

Table 1: Land Use Summary

Lot/ Block	Land Use	Area m ²	% Site Area	Units
1-29	Single detached residential units	14892.7	12.5	29
30-39 & 46-48	Semi-Detached Lots	8976.6	7.6	26
40-45 & 49-54	Freehold Townhouses	13336.4	11.2	54
55-60	10-Plex Units	30456.7	25.7	122
61	Apartment Block	14172.8	11.9	102
62	Stormwater Facility	7558.1	6.4	
63	Parkland	2398.4	2.0	
64	King Street Road Widening	668.1	0.6	
65	Willmott Street Road Widening	3675.8	3.1	
20.0m Road Allowance	(Extension of Maplewood Blvd., Hayward Street, Drewery Road	22569.1	19.0	
Site Total		118704.7m ²	100%	333

Figure 3 – Development Site Plan illustrates the proposed internal roads, parkland and stormwater blocks, drainage easement, sidewalks, applicable building setbacks, and potential building footprints and parking lot configurations for the medium density units. The subdivision has been assessed against the Zone provisions of By-law 85-2003 and the existing site-specific zone provisions for Phases 1-4.



LAND USE SUMMARY			
LAND USE	AREA(m ²)	AREA%	UNITS
LOTS 1-29 - SINGLE DETACHED LOTS	14892.7	12.5	29
BLOCKS 30-39 - 46-48 - SEMI-DETACHED LOTS	8976.6	7.6	26
BLOCKS 40-45 & 49-54 - FRESHLOD TOWNHOUSES	13336.4	11.2	54
BLOCKS 55-60 - IO PLEX UNITS	30456.7	25.7	122
BLOCK 61 - APARTMENT BLOCK	14172.8	11.9	102
BLOCK 62 - PARK WATER FACILITY	7598.1	6.4	
BLOCK 63 - STORM DRAINAGE	2398.4	2.0	
BLOCK 64 - KING STREET ROAD WIDENING	0.6	0.001	
BLOCK 65 - ROAD WIDENING, WILLMOTT ST. ALLOWANCE	3676.9	3.1	
20.0m MUNICIPAL ROAD ALLOWANCE = 1140.1m (KING STREET, WILMOTT STREET AND DREWERY ROAD)	22569.1	19.0	
TOTAL	118704.7m²	100.00%	333

ADDITIONAL INFORMATION REQUIRED
UNDER SECTION 51(17) OF THE
PLANNING ACT.

- (a) SEE SURVEYORS CERTIFICATE.
- (b) AS SHOWN ON DRAFT PLAN.
- (c) AS SHOWN ON DRAFT PLAN.
- (d) SEE LAND USE SUMMARY
- (e) SEE DRAFT PLAN.
- (f) AS SHOWN ON DRAFT PLAN.
- (g) AS SHOWN ON DRAFT PLAN.
- (h) MUNICIPAL WATER AND SANITARY SEWER
- (i) SAND AND SILT TILL
- (j) AS SHOWN ON DRAFT PLAN.
- (k) GARBAGE COLLECTION FIRE PROTECTION AND SCHOOL BUSES
- (l) AS SHOWN ON DRAFT PLAN.

SURVEYOR'S CERTIFICATE
 I CERTIFY THAT THE BOUNDARIES OF THE LAND TO BE SUBDIVIDED ARE
 CORRECTLY SHOWN.

C. Grant Dec 17/18
 CRYSTAL CRANCH DATE
 ONTARIO LAND SURVEYOR.

 I.B.W. SURVEYORS
 ONTARIO LAND SURVEYORS
 71 MEARNS COURT, BOWMANVILLE, ONTARIO
 (905) 667-0888

No.	REVISION	DATE	APPR'D
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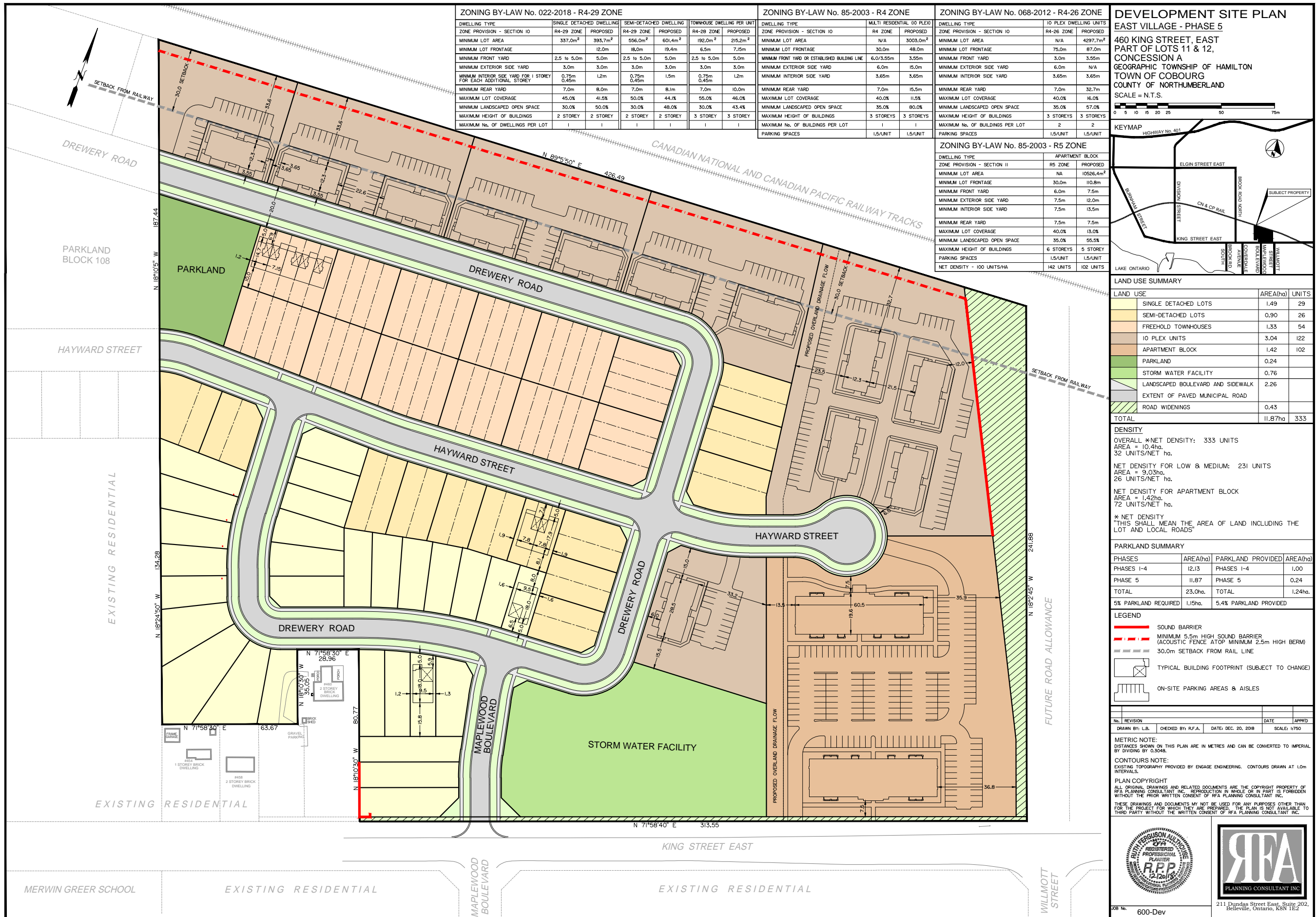
METRIC NOTE:
DISTANCES SHOWN ON THIS PLAN ARE IN METRES AND CAN BE CONVERTED TO IMPERIAL BY DIVIDING BY 0.3048.

CONTOURS NOTE:
EXISTING TOPOGRAPHY PROVIDED BY ENGAGE ENGINEERING. CONTOURS DRAWN AT 1.0m INTERVALS.

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3.2 Subdivision Road Network and Traffic Impact

The main entrance to the Phase 5 subdivision will be from King Street East at the Maplewood Boulevard intersection. There will be road connections to Hayward Street and Drewery Road at the limit of Phase 4. There will also be a driveway on King Street East to serve the apartment block. Other potential road connections to adjoining properties on King Street were reviewed and dismissed as being impractical due to existing property configuration and site grading.

The proposed right-of-way width is 20 metres; the total asphalt pavement width will be 8.5 metres. A 1.5 metre-wide sidewalk will be constructed on both sides of the right-of-way. The municipal boulevard will be landscaped with street trees according to the requirements of the Town. A typical road cross-section is in the Engage Functional Servicing Report. Road widening will be deeded to the Town as a condition of draft plan approval of Phase 5 along King Street East and for the future extension of Willmott Street.

TranPlan prepared a Traffic Impact Study that updates a February 2008, traffic study by MMM Group for Phases 1-5. The study found that the Phase 5 subdivision will have only minor impact on the intersections in the vicinity. The proposed road access onto King Street East via Maplewood Blvd. as well as the driveway for the apartment block, will all provide good levels of service.

The study concluded that an eastbound left turn lane on King Street East at Maplewood Blvd. may be warranted. This will be determined after incorporating the full build-out of the study site and as part of the planned improvements to King Street East, which is recommended for widening to four-lanes (including bicycle lanes) in the 2021-2031 planning horizon in the Transportation Master Plan.

3.3 Site Servicing

Municipal water and sanitary sewer services are proposed to service the subdivision. A Functional Servicing Report has been prepared by Engage Engineering in support of the planned services. The FSR reviewed existing municipal services to confirm that water, sanitary sewer and utilities are sufficient to service the subdivision, and that the distribution network and treatment plants are also sufficient. No issues are foreseen in providing services for the subdivision.

Engage Engineering has analysed how to mitigate the increase in stormwater runoff and minimize sediment loading or flooding downstream from the site. They

have met and consulted with Town and GRCA staff, and followed the current best management practises, design guidelines and technical requirements.

A wet pond is proposed to provide quantity and enhanced level of quality control. The wet pond will also act as an amenity feature at the entrance to the subdivision complete with a pathway around the perimeter and landscaping around the top and side slopes of the pond. The gentle side slopes were designed in accordance with GRCA standards to eliminate the need for fencing. A landscaping plan will be developed for the pond and surrounding area during the detailed design stage. There will also be low impact development (LID) measures incorporated into the subdivision design to promote groundwater recharge and improve stormwater quality and quantity. These will take the form of infiltration trenches and soakaway pits. The LID's will allow rear yard drainage and rooftop runoff to re-charge groundwater on-site.

The SWM facility will control the release and duration of run-off to less than the rates of runoff before development. Sufficient storage is provided in the pond to enable offsetting of the peak flows for up to the 100-year storms. There is an overland flow route identified on the draft plan of subdivision. It is an easement that is 8 metres in width and will allow the conveyance of overland drainage to occur on the site in the event of any blockage at the site of the SWM pond. There will be erosion and sediment controls in place prior to construction occurring.

3.4 Parkland

A block for parkland has been designated in Phase 4 of the East Village subdivision (Block 108, Plan 39M-875). It is approximately 1.0 ha in size. The Town planning staff requested that Phase 5 provide a parkland block adjacent to Block 108 to ensure that 5% of the overall East Village community has a designated local park. Block 63 on the draft plan will be deeded as park to the Town; it is adjacent to Block 108. It is approximately 0.24 ha in area with frontage on both Drewery Road and Hayward Street. Overall and combined with the parkland to be deeded in Phases 1-4, there will be just over 5% of parkland provided or 1.24 ha. for the entire 23 ha. of the East Village community.

3.5 Potential Environmental Impacts

An Environmental Impact Study (EIS) and species at risk (SAR) screening was prepared by Cambium in September 2018. The EIS addresses all requirements of the PPS, County and Cobourg OP, GRCA regulations, Endangered Species Act,

Species at Risk Act and Fisheries Act. Consultations were held with MNRF, and there were 5 site visits in the spring and summer of 2018 to conduct field surveys.

No species at risk were identified on-site and potential impacts related to the subdivision and site alteration are expected to be minimal. The EIS recommends implementation of erosion and sediment controls, avoidance of clearing and grubbing during the main breeding season for birds (April- August), scanning for swallow nests and other wildlife (turtles and snakes) during construction, incorporation of LID stormwater features, preparation of a landscape restoration plan, and habitat compensation for the Monarch.

Phase 1 and Phase 2 Environmental Site Assessment Reports were completed by GHD. The only previous identified land use of the property is agricultural. Because there was imported fill in stockpiles and active adjacent rail lines, the consultant recommended soil testing be undertaken and the installation of monitoring wells. All of the soil and groundwater samples were tested and met all provincial standards. The consultant determined that the site is suitable for future residential uses.

Valcoustics updated the Environmental Noise Analysis that was originally prepared in 2005. The noise sources are the CP and CN rail traffic and road traffic on King Street. They have made recommendations regarding mandatory or future air conditioning for buildings; brick/ masonry/ upgraded wall construction; upgraded exterior windows; and warning clauses. They have identified that 5.5m high sound barriers or acoustic fencing will be required along the north, north-east and south-west exterior property lines. The barrier will consist of an acoustic fence on top of a 2.5m high berm. There will be a minimum 30m building setback from the rail line.

4.0 PROVINCIAL POLICY STATEMENT

The Provincial Policy Statement (PPS) provides policy direction on matters of provincial interest related to land use planning and development. All decisions related to land use planning matters “shall be consistent with” the PPS. The relevant policies from the 2014 PPS are summarized below and are attached in **Appendix A**. The applications for a plan of subdivision, Official Plan Amendment and Rezoning are consistent with the 2014 PPS.

The subject property is within a designated settlement area of the municipality, which shall be the focus of growth (1.1.3.1). Within settlement areas, land use patterns shall be based on densities that efficiently use land and resources, and are appropriate for and efficiently use the infrastructure which is available (1.1.3.2) without the need for unjustified or uneconomical expansion; and which support active transportation. The development is adjacent to an existing built-up area and is a compact form that allows for the efficient use of available land and infrastructure (1.1.3.6). The project is a subdivision that will provide a mix of housing types and densities to meet projected requirements of current and future residents of Cobourg and area (1.4.1). Existing infrastructure and public service facilities will support this subdivision (1.4.3).

Sidewalks on the proposed road network will connect into the existing neighbourhood. This will promote healthy active communities by facilitating the movement of pedestrians. The draft plan will extend Maplewood Boulevard into the subdivision which will provide access to the existing and planned pedestrian/bicycle path along King Street East, thereby fostering community connectivity (1.5.1).

The proposed subdivision is consistent with Section 1.6 of the PPS; municipal sewage and water supply will service the subdivision and are the preferred form of servicing in settlements areas (1.6.6.2). It will optimize the use of existing infrastructure. The draft plan will prevent increases in contaminant loads and volume of run-off through the design of the stormwater management facilities (1.6.6.7) and (2.2.1h). The Plan provides a modified rectilinear road network which will efficiently move people and vehicles. This will ensure safe egress into the subdivision (1.6.7.1). The draft plan will support energy conservation and efficiency through land use design, orientation, and vegetation (1.8.1).

The protection of natural heritage, surface water and ground water features have been addressed through the preparation of an Environmental Impact Statement

(EIS) and Stormwater Report. Within the EIS report, the relevant sub-sections 2.1 and 2.2 of the PPS were considered. The EIS concluded that potential negative impacts associated with the proposed development and site alteration are expected to be minimal. The study provides recommendations to ensure that development will not adversely affect natural heritage features and functions on or adjacent to the subject site.

5.0 GROWTH PLAN FOR THE GREATER GOLDEN HORSESHOE

On May 18, 2017 the Growth Plan for the Greater Golden Horseshoe was released and came into effect on July 1, 2017, replacing the 2006 Growth Plan. The Growth Plan is a long-term plan that works together with the Greenbelt Plan, the Oak Ridges Moraine Conservation Plan and the Niagara Escarpment Plan to manage growth, build complete communities, curb sprawl and protect cultural heritage resources and the natural environment. The Growth Plan, which includes the County of Northumberland and the Town of Cobourg, sets out how the Greater Golden Horseshoe (GGH) will grow in the coming decades. Applicable extracts are in **Appendix B**.

The Growth Plan provides policies for where and how to grow, directing population and employment growth to urban areas and rural settlement areas within delineated built boundaries, on full municipal services, that can support complete communities (policy 2.2.1.). The policies of the Growth Plan are to be applied to support complete communities that feature a diverse mix of land uses with convenient access to local stores, services, and public service facilities; improve social equity and overall quality of life; provide a diverse range and mix of housing options; expand convenient access to transportation options, public service facilities, accessible open space and healthy local, affordable food options; ensure development of high quality compact built form; mitigate and adapt to climate change impacts, and integrate green infrastructure and low impact development (policy 2.2.1.4 a-g). Policy 2.2.2.1 directs that a minimum of 60% of all residential development occurring annually will be within the delineated built-up area.

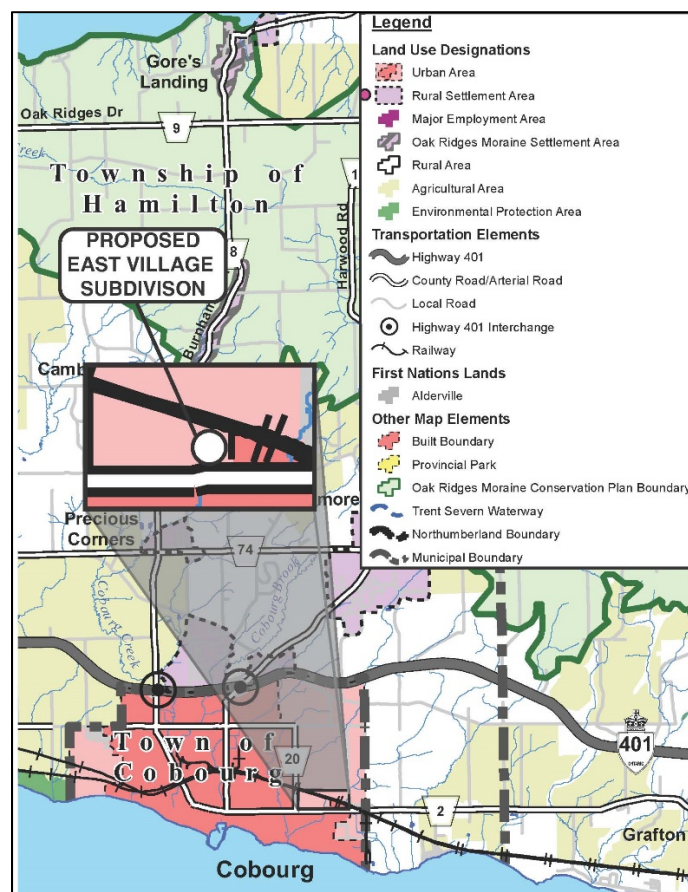
All decisions made on or after July 1, 2017 in respect of the exercise of any authority that affects a planning matter, are required to conform to the 2017 Growth Plan. In reviewing the Growth Plan, it was found that the intent of the relevant policies has been maintained. Although not inside the built boundary, the development is immediately adjacent to the delineated built boundary. This project will make efficient use of land, existing infrastructure and public service facilities.

6.0 COUNTY OF NORTHUMBERLAND OFFICIAL PLAN

The County of Northumberland Official Plan was approved by the OMB on November 23, 2016. The lands are designated “*Urban Area*” on Schedule ‘A’ – Land Use Plan of the County of Northumberland Official Plan. The East Village subdivision conforms to the policies of the Northumberland County Official Plan.

The purpose of the County of Northumberland Official Plan is to manage growth and change in accordance with the Provincial Growth Plan and the needs of the greater community. The subject lands are located immediately adjacent to the built boundary of the Urban Settlement Area of Cobourg on Schedule “A” to the County of Northumberland Official Plan. Relevant extracts are attached in **Appendix C** of this report.

Figure 4: Extract of County Official Plan Designation



The Growth Plan anticipates an overall population increase in the County of 25,000 people between 2011 and 2041 (A1); this future growth is then allocated by the County OP to the settlement areas within the County of Northumberland; almost half of all the County's future urban area population and employment growth is forecast to occur in Cobourg (B2 –Tables A and B). This growth is required to occur within the designated urban settlement area of Cobourg. The subject property is within the urban area and immediately adjacent to the built boundary.

In terms of population growth, an increase in population of 7000 persons is forecast for Cobourg by 2034 (B6 Table G) with a corresponding increase in housing units of 3096 (B7 Table H); 30% of the total new housing units for the Town will be low density units, 42% for medium density units and 28% for high density units.

The County's *Urban Areas* are to be the focus of growth with a minimum of 80% of the population expected to occur there (Section B2). Table J in Section B10 indicates that the minimum density target within the Greenfields of the Cobourg Urban Area is 35 people and jobs per gross hectare.

- The development will be built at a density of 28 units/gross hectare which would result in approximately 70 persons per gross hectare, based on an average of 2.5 persons/unit. This exceeds the minimum density target for the municipality. The allocation of housing form approximates the forecasts identified on Table B and the mix of density on Table H.

Section *C1.2.1 General Land Use Objectives for Residential Areas* sets out the objectives of the Official Plan for *Residential Areas* that are situated in *Urban Areas*. The proposed subdivision would satisfy these objectives as summarized below:

- a) Maintain and enhance the character and identity of existing residential areas;*
 - The existing residential area consists of a mix of housing forms including singles, semis, townhouses and walk-up apartments. The proposed subdivision will be designed to integrate into the previously approved phases of the East Village subdivision. The proposed housing forms will include singles, semis, townhouses, walk-up apartments and apartment buildings. The apartment buildings will be located away from the low density uses and adjacent to King Street East. The builder of the previous phases of the subdivision (Stalwood) will continue in Phase 5 with the new urbanism building style.
- b) Encourage the provision of a range of housing types to accommodate persons with diverse social and economic backgrounds, needs and desires while promoting the maintenance and improvement of existing housing;*

- The subdivision includes a wide variety of housing forms including singles, semi, townhouses, walk-up apartments and apartment buildings. This combination of units will contribute towards the range of housing types available to meet the needs and desires of residents within Northumberland County.
- d) *Encourage increases in density in new development areas to maximize the use of infrastructure and minimize the amount of land required for new development;*
- The development provides an overall density of 28 units per gross hectare (70 persons per gross hectare) and exceeds the minimum density target for Greenfields in Cobourg.
- e) *Promote a variety of complementary and compatible land uses in residential areas including special needs housing, community facilities, schools, small-scale commercial uses and recreational open space areas;*
- Some of the high density residential units may be used to support seniors' housing needs. Parkland will be provided in the north-west portion of the subdivision. This will be designed to be an extension of the parkland located in the subdivision to the west. Additional open space will be provided on lands that will be used for a stormwater facility. The subdivision is close to schools and retail and service commercial uses are planned for the south-east corner of Willmott and King St. E.
- f) *Encourage a high standard of urban design for development and redevelopment.*
- The builder incorporates new urbanism design into their plans. Porches and recessed garages are provided which helps to ensure a more street-oriented housing design.
- h) *Implement street designs that provide for pedestrian, cycling and other non-motorized modes of transportation to help create healthier and complete communities.*
- Sidewalks will be built on both side of all streets within the subdivision and will connect to the existing sidewalk network. Within the apartment block, sidewalks will be provided to move pedestrians from the parking area to the building.

The County Housing Policies are in located in Section C1.5.1. While many of these are municipally oriented, the proposed subdivision will help meet these goals by (a) ensuring that there is a 10-year supply of land for residential development with

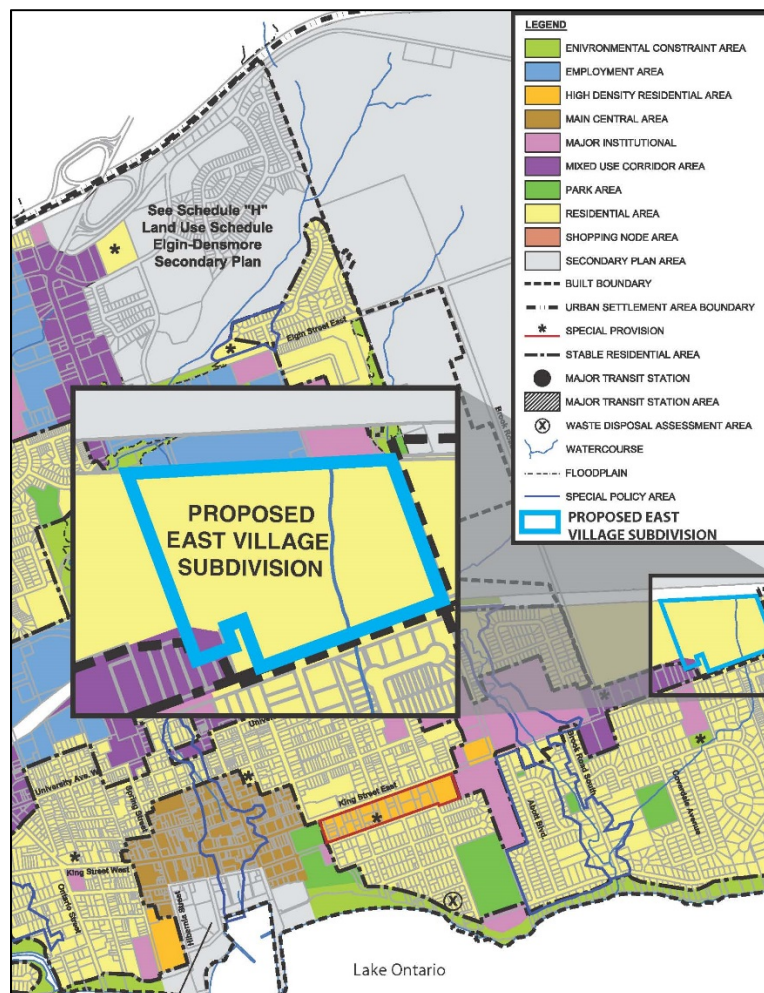
sufficient water and wastewater capacity; and (b) ensuring the provision of an appropriate range of housing types to meet the needs of current and future residents. The 333 units will assist in providing a variety of housing forms to meet the municipality's housing needs.

The Stormwater Management policies in Section D2.4 are intended to control flooding, erosion, sedimentation and to protect water quality and aquatic habitat. It requires storm water management plans for any new subdivision according to the 2013 MOECC Design Manual. The use of SWM facilities downstream of new developments are required to mitigate development impacts on storm water quantity and quality. A Stormwater Management Report has been prepared by Engage Engineering Ltd. to ensure water quality and quantity control during and after construction.

7.0 TOWN OF COBOURG OFFICIAL PLAN

Since the original submission of the East Village planning applications, the 2010 Five Year Review of the Cobourg Official Plan has been approved. The specific land use planning policies for the subdivision lands are essentially unchanged by the Five Year Review, however there are now additional policies for community design and sustainability that are addressed in Section 8.0 of this report. The relevant extracts from the Official Plan are found in **Appendix D**. As with the County Official Plan, the subject land is located immediately adjacent the built boundary of the urban settlement area on Schedule 'A' Land Use Plan and is designated "Residential Area".

Figure 5: Extract of Cobourg Official Plan Designation Schedule A



Section 3.2.2(ii) Designated Greenfield Area states: *The lands between the Built Boundary and the Town's Urban Settlement Area Boundary as designated on Schedule "A" are considered "designated greenfield areas". These are lands are largely undeveloped. Development in these areas are required to contribute to the achievement of the minimum designated greenfield area density target in Section 3.2.3 of the Plan.*

- The subject property is considered to be a greenfield area given its location between the Built Boundary and the Urban Settlement Area Boundary.

There is some discrepancy in the Town OP growth management housing target, as the Town has targeted 65% of the housing to be single-detached housing (vs. 30% in County OP) and 35% multiples. The reason for the discrepancy is because the Town adoption and Ministry approval of the Cobourg Official Plan was in 2010/11; it pre-dated the more recent County growth management policy. Until the Town's OP is updated again, the County OP policies are considered more accurate and a reflection of relevant housing and population targets.

Section 3.2.3 (iv) refers to the target density in greenfield areas: *In the Town's designated greenfield area as a whole, the density target is a minimum of 30 persons and jobs per hectare. This target is not intended, however, to be considered a minimum target on every parcel of land. In addition, increases in density for individual developments which conform to the other policies of this Plan shall be permitted.*

- The density of the subdivision is 28 units/gross ha which equates to a density of approximately 70 persons/ha and therefore meets and exceeds the desired minimum density.

Section 3.4.2 provides policy regarding permitted uses in the "Residential Area" designation: *The permitted uses, buildings and structures are:*

- i) low density residential including single detached, semi-detached and duplex dwellings; and,*
- ii) medium density residential including townhouse dwellings, low rise apartments and stacked townhouses.*

- The proposed housing forms include single detached units, semi detached units, townhouses, 10-plex apartments and 5-storey apartment buildings. All uses are permitted in the "Residential Area" designation except for the 5 storey apartment buildings. This use is allowed in the "High Density Residential" designation.

Section 3.4.3.2 includes policies regarding residential development in new residential areas. *In new residential areas or significant redevelopment areas, applications for development shall be evaluated based on their conformity with the Growth Management Strategy in Section 3.2 and all other applicable policies of this Plan and the following criteria:*

i) a mix of development forms and densities;

- The development housing forms to be located in the subdivision include single-detached units, semi-detached units, townhouses and 10-plex apartments. The density of the development has both low and medium density housing forms.

ii) medium density residential uses are encouraged and shall be:

a) intermixed with low density development in smaller groups;

b) primarily street oriented in design; and,

c) located adjacent to collector and arterial roads, park and greenland areas, community facilities and commercial areas and/or as a physical transition between high and low density residential development.

- The medium and low density uses are intermixed, street oriented and are located on new local streets that have access to King Street, an arterial road and are located in proximity to parkland and primary and secondary schools. The more sensitive low density residential uses are positioned adjacent to existing single-detached homes farthest away from the CP Railway. The medium density uses provide a transition between the low density units and the proposed apartment buildings located in the south-east corner of the subdivision.

iii) the road pattern is a modified, rectilinear grid pattern which provides for the maximum possible degree of connectivity internally, and externally with the existing developed areas and abutting arterial and collector roads with short blocks to promote active transportation modes;

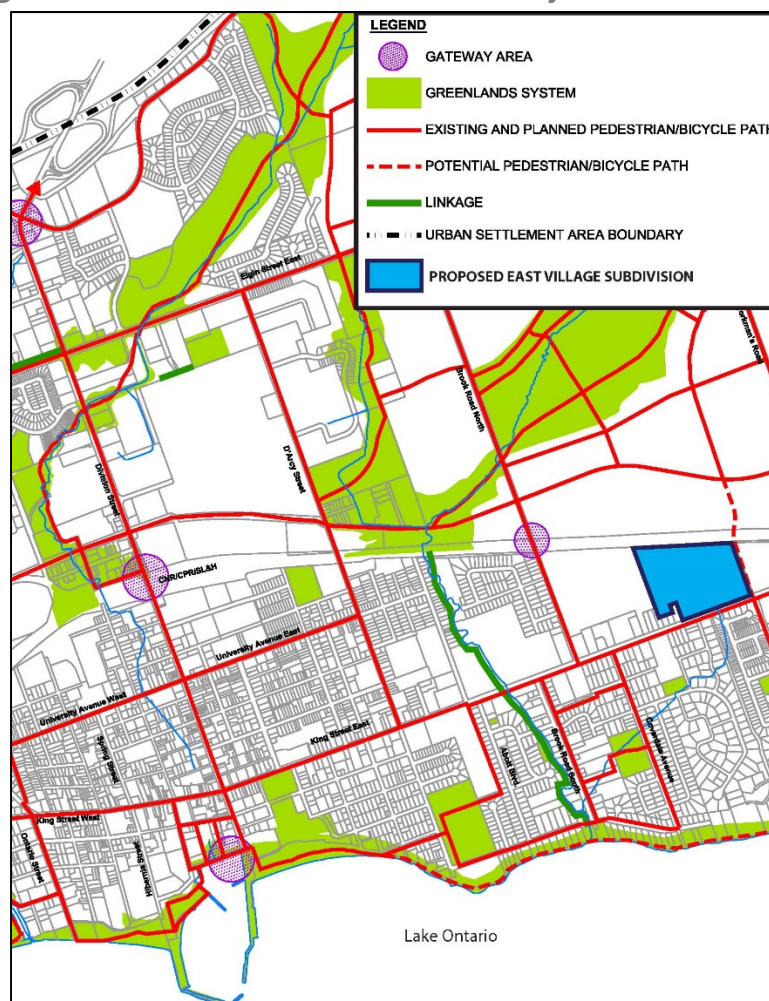
- The road network has been designed to be a modified rectilinear pattern to allow for efficient movement of vehicles and pedestrians. The road network has been designed to connect into adjacent phases of the subdivision located to the west. Drewery Road and Hayward Street will connect into the subdivision to the west and Maplewood Boulevard will provide external connectivity onto King Street East, an arterial road.

iv) the development incorporates linkages to the Town's greenland system and, incorporates private or public open space features or areas including Village Squares which serve as focal points for the residential development and/or structural elements which define the character and structure of the area.

- The subdivision includes 0.2 ha of open space parkland that will be an extension of the open space parkland deeded in Phase 4 of the East Village subdivision to the west. A stormwater pond, with frontage on King Street, will provide visual open space at the entrance to the subdivision.

Official Plan Schedule 'B'- Greenland System and Gateway Area and Schedule 'E'- Transportation Plan show that King Street consists of an existing and planned pedestrian/bicycle path. The proposed road pattern will directly link the subdivision with pedestrian/bicycle paths and the Town's Greenland system overall. In addition, the walkway proposed through the local park and the subdivision to the west will connect the proposed subdivision to Brook Road North, which also has existing and planned pedestrian /bicycle paths.

Figure 6: Extract of Greenland/Gateway Area Schedule B



The density requirement found in Section 3.4.3.3 are as follows:

The density ranges for residential development shall be:

i) Low Density

a) 12 units per net hectare (5 units per net acre) minimum

b) 20 units per net hectare (8 units per net acre) maximum

ii) Medium Density

a) 20 units per net hectare (8 units per net acre) minimum

b) 50 units per net hectare (20 units per net acre) maximum.

- The total number of housing units is 333. The overall net density (as defined by the OP) of the subdivision is 32 units/net ha. The portion of the development accommodating single, semis, townhouses and walk-up apartments has a net density of 26 units/ha. The area which will accommodate only the two apartments will be built at a net density of 72 units/ha. Although the overall net density of the subdivision is within the lower range for medium density, an Official Plan Amendment is proposed to permit the 5-storey apartment buildings. The designation of Block 61 on the Draft Plan will be amended from “Residential Area” to “High Density Residential Area”.

Section 3.4.3.4 refers to building height. *The maximum height for residential development shall be three storeys in Stable Residential Areas and four storeys in New Residential Areas.*

- The development is located within a New Residential Area. The height of the low and medium density housing forms will not exceed three storeys. The apartment block will be 5 storeys, so it exceeds the height limitation for medium density uses.

Section 3.5 addresses uses permitted in the “High Density Residential Area”.

The permitted uses, buildings and structures are:

i) medium density residential including townhouse dwellings, low rise apartments and stacked townhouses; and,

ii) high density residential including high rise apartments.

- The five storey apartment building is permitted in the high density designation.
- A retirement home/ seniors housing is also proposed to be added as a potential permitted use on the high density block. It has been confirmed by Town Planning staff that a retirement home or seniors’

assisted housing would also be allowed in this designation. The OPA may specify that seniors and supportive housing is allowed on the high density block.

Section 3.5.3.2 provides policies regarding the High Density designation.

Applications for new High Density Residential Area designations shall be evaluated based on their conformity with the Growth Management Strategy in Section 3.2 and other applicable policies of this Plan and the following criteria:

i) a mix of development forms and densities;

- The apartment buildings are part of a subdivision with mixed housing forms.

ii) high density residential uses are:

a) intermixed with medium density development;

b) primarily street oriented in design; and,

c) located with direct access to collector and arterial roads, park and Greenland areas, community facilities and/or commercial areas.

- The high density use is intermixed in a subdivision containing low and medium density uses; the buildings are oriented towards two streets, Hayward Street and King Street East; and will have direct access to King Street East, an arterial road. The site is in proximity to elementary and secondary schools, parkland and planned bicycle paths. In addition, the land at the southeast corner of King Street East and Willmott Street have been approved for a food store and a 20,000 sq. ft. commercial plaza.

iii) designed to ensure that there are no significant negative impacts with respect to privacy and shadowing, and that appropriate buffering can be provided for any adjacent lands in the Residential Area designation;

- The apartment block is separated from low density uses by existing/future arterial streets and the stormwater block. Substantial setbacks can be provided to ensure privacy and minimize shadowing. A significant amount of the proposed parking area will separate the two apartment buildings. This will minimize and/or eliminate the effects of shadowing. A landscaping plan will form part of the site plan approval process and will identify any necessary buffering.

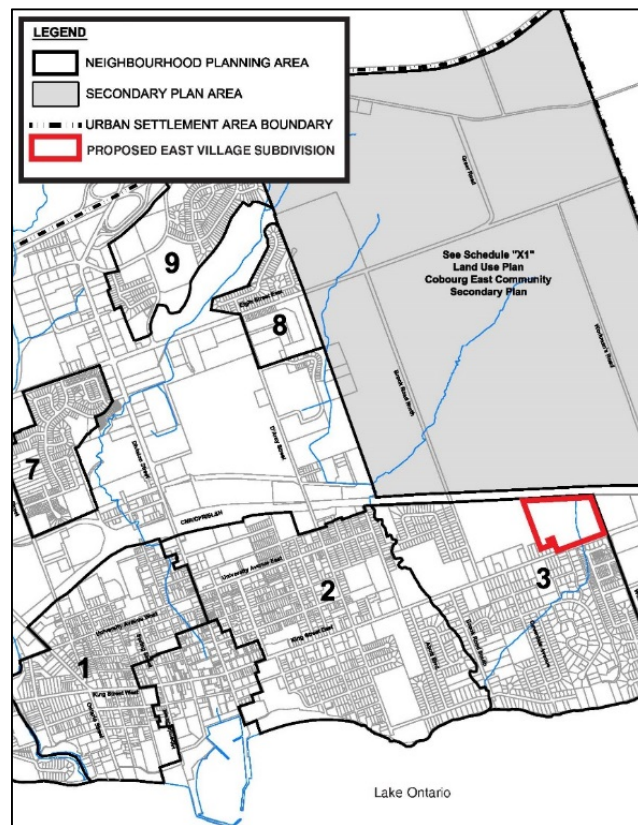
iv) size and scale of the development is such that it can be integrated with any adjacent residential areas, in particular conforms with the policies of Section 5.5, Cultural Heritage Preservation and preserves designated and listed cultural heritage buildings and structures, and where located adjacent to such buildings and structures is designed to be compatible.

- The development will be progressively more dense towards the north and east sides of the property. Ten-Plex buildings and the stormwater management facility will buffer the high density use from the more sensitive low density uses.

Section 3.5.3.3 states that the range of density within the High Density Residential Area shall be between 50 units per net hectare and 100 units per net hectare. The proposed apartment complex will be developed at a net density of 72 units per hectare and is therefore within the allowable density range for the designation. Section 3.5.3.4 states that the minimum height shall be three storeys and the maximum height shall be six storeys. The apartment building is proposed to be five storeys and is therefore permitted in the designation.

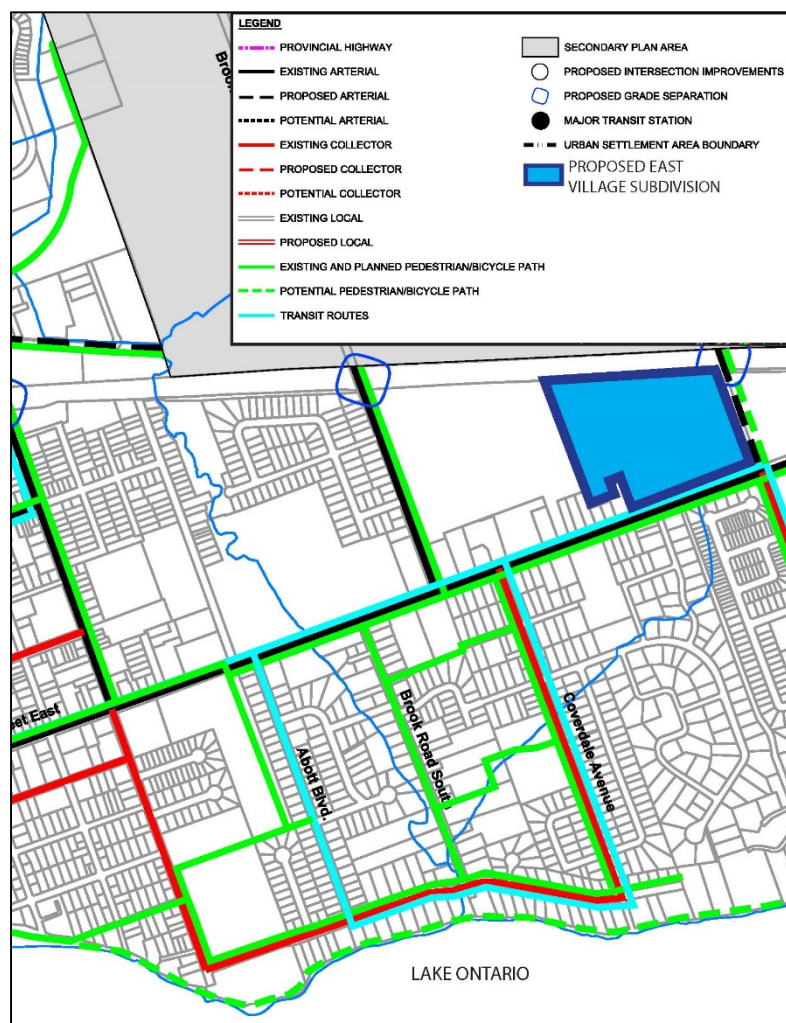
Figure 7: Extract of Neighbourhood Planning Area Schedule C

Section 5.4 provides policies regarding Neighbourhood Planning Areas. The subdivision is located in Neighbourhood Planning Area No. 3 on Schedule C. The Areas are primarily composed of low and medium density forms of housing, although in some cases there is the provision for the development of high density housing forms. The Neighbourhood Planning Areas are largely developed. The policies also indicate that prior to development of undeveloped portions of the Area, the Town may require the preparation of a Secondary Plan and/or a number of studies. These studies include: servicing, stormwater management, transportation, environmental audit and concept plan. All of the proposed uses are permitted in the Neighbourhood Planning Area. The Town did not require a Secondary Plan; however, all the required studies listed in Section 5.4 have been completed and form part of the Planning applications submission.



Section 6.2, Road Network includes Table 3, Function of Road Facilities. All proposed road extensions will be classified as Local Roads. The roads will serve local and neighbourhood travel demands and will connect into existing neighbourhoods. Roads and sidewalks will efficiently move vehicles, cyclists and pedestrians. The local road network will connect the subdivision onto North Brook Road and King Street East, both arterial roads. The right-of-way width should be a maximum of 20 metres. All rights-of-way are 20 metres wide in the draft plan, and sidewalks will be provided on both sides of all internal streets. The draft plan provides blocks for future road widening of King Street East and Willmott Street North, including the future grade separation at the CP/CN rail line, as shown on the Transportation Schedule E of the Official Plan.

Figure 8: Extract of Transportation Plan Schedule E



Section 6.3.2 states that: “*new subdivisions shall generally be located as close as possible to existing transit service.*”

- An existing transit route is located on King Street East immediately south of the subdivision.

Water Supply and Distribution System Capacity, Section 7.2.1, states: “*New development and/or redevelopment shall only be permitted where the water supply and distribution system has adequate capacity to service such development and/or redevelopment.*”

- The Functional Servicing Report prepared by Engage Engineering Ltd. states that the proposed development will not create a capacity constraint for the Cobourg Water Treatment Plant and that water flows to the development can be accommodated within the current water system capacity.

Municipal Sewage Collection and Treatment System Capacity, Section 7.3.1, indicates: “*New development and/or redevelopment shall only be permitted where the sewage collection and treatment facilities are adequate to service such development.*”

- Engage Engineering Ltd. prepared a report that states that the existing sewer infrastructure has adequate capacity to accept flows from the development. In addition, there is residual capacity in the sewer system to accept additional flows in the future.

Stormwater Management Facilities for New Development, Section 7.4.3, states that: “*stormwater management facilities shall be designed in a naturalized manner and will be landscaped so that they are integrated with the surrounding area and form part of the Greenlands System.*” The Official Plan indicates that the primary focus for water quality shall be enhanced treatment. Stormwater management practices should be evaluated to best meet water quality objectives. This includes appropriate preliminary design, siting and sizing of proposed facilities. With respect to quantity control, development shall comply with the requirements of an approved Master Drainage Plan or stormwater plan. Any stormwater management facility shall be located entirely within the Municipality. Individual lot level approaches are encouraged on all sites to reduce runoff volume and to treat stormwater on-site through the implementation of Low Impact Development approaches including green roofs and porous or permeable pavement.

- Engage Engineering Ltd., has determined that given the proposed conditions, existing storm sewer infrastructure downstream has adequate capacity to accept flows from the development. The SWM plan that is proposed conforms to all of the policies of Section 7.4.3.

Section 8 of the Official Plan requires that all development applications requiring Planning Act approval be subject to consultation with the Town prior to submission. Specific materials and information will be required to be submitted as part to the applications. At the pre-consultation meeting with the Town on October 10, the staff provided the applicants with the list of technical reports required to support the submission.

Section 8.5.6 states that an archaeological assessment be carried out as part of development applications. An archaeological assessment has been satisfactorily completed for the entire holding.

Section 9.3 outlines policies regarding Official Plan amendments:

In considering an amendment to the Plan, regard shall be had to the following criteria which are in addition to those specified in the remainder of this Plan:

i) The need for the proposed use;

- The proposed apartment complex will assist the Town in meeting the high density residential targets which are outlined in the Town's Official Plan, Northumberland Official Plan and the Growth Plan.

ii) The extent to which the existing areas in the proposed designations are developed, and the nature and adequacy of such existing development;

- There are only a few areas within the Town that are designated as High Density Residential Area; there are no identified locations within the southeast portion of Cobourg. The majority of the lands that are designated for a high density use in the Town are already built out.

iii) The physical suitability of the land for such proposed use;

- The land is flat and well drained and is of sufficient area and frontage to accommodate a high density uses.

iv) The location of the areas under consideration with respect to:

a) the adequacy of the existing and proposed road system in relation to the development of such proposed areas;

b) the convenience and accessibility of the site for vehicular and pedestrian traffic and traffic safety in relation thereto; and

c) the adequacy and availability of the municipal water and sewage facilities, and other municipal services in view of the policies contained in this Plan and in accordance with technical reports or recommendations submitted by the applicant or agencies.

- The Official Plan states that High Density Residential Areas have access to an arterial road. The site has frontage on and will have a driveway access onto King Street East, an arterial road. The traffic study indicates that entry/exit into the subdivision from King Street

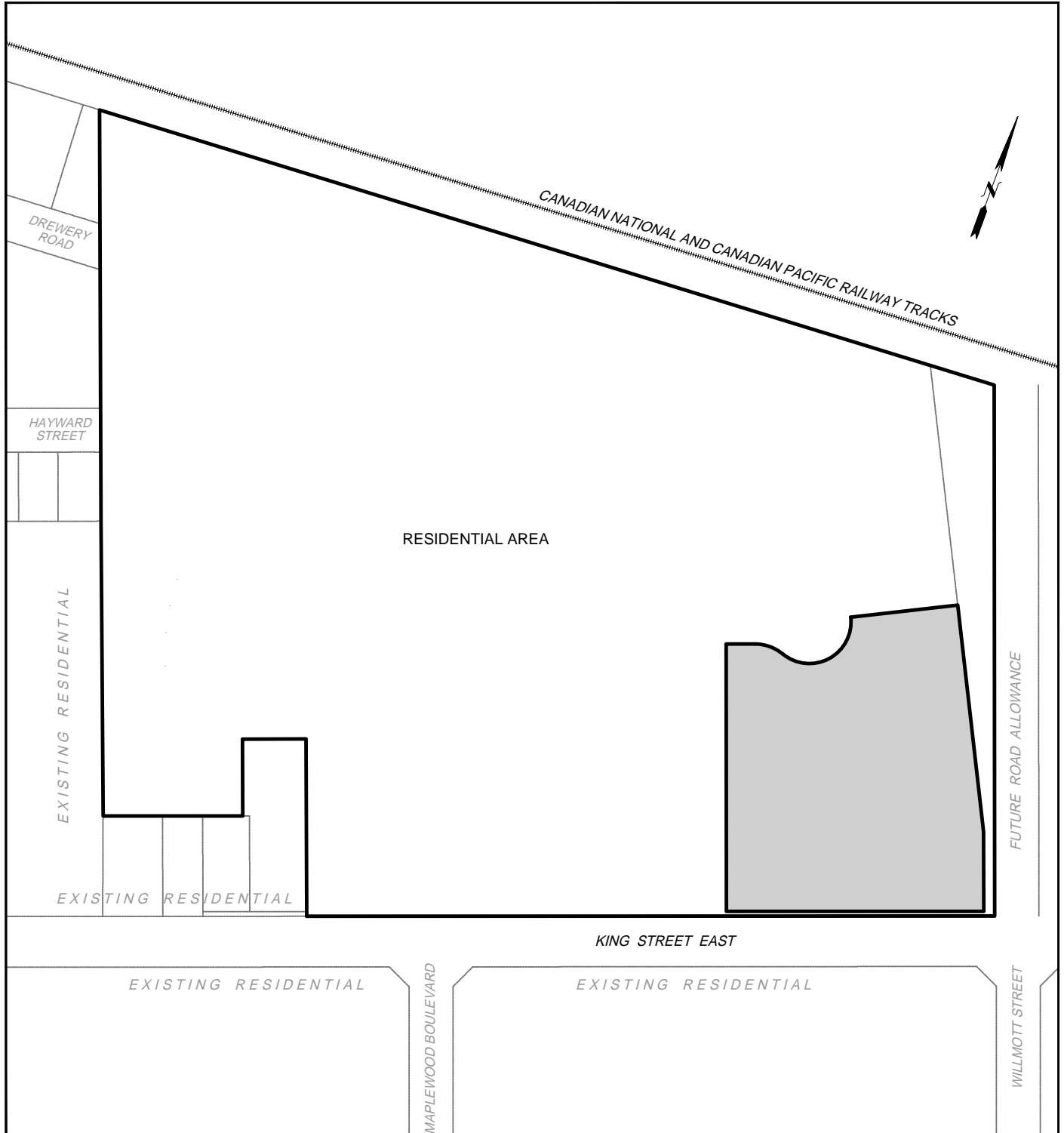
East is appropriate. The existing road network and sidewalks will extend into the subdivision. Full municipal services are readily available to the site as outlined in the submitted technical reports.

v) The compatibility of such proposed use with uses in the surrounding area; and,

- The proposed use can be developed in accordance with the High Density Residential Area policies of the Plan. It is located on an arterial road which can accommodate relatively high volumes of traffic. The high density use will be buffered from lower density uses and is separated by medium density 10-plex buildings and the stormwater facility to provide a transition in density. Landscaping will further buffer the proposed use.

vi) The potential effect of the proposed use on the financial position of the Town.

- The site will make efficient use of existing municipal services. The 102 additional residential units will increase the Town's tax revenue and provide more affordable housing for smaller households and seniors.



LEGEND:



LANDS TO BE DESIGNATED FROM
"RESIDENTIAL AREA" to
"HIGH DENSITY RESIDENTIAL AREA"

LOCATION

EAST VILLAGE - PHASE 5
PART OF LOTS 11 & 12,
CONCESSION A
GEOGRAPHIC TOWNSHIP OF HAMILTON
TOWN OF COBOURG
COUNTY OF NORTHUMBERLAND

OFFICIAL PLAN AMENDMENT —
TO THE OFFICIAL PLAN
SCHEDULE '1'



SCALE: 1:3000 DATE: DECEMBER 20, 2018
0 10 25 50 75 100 125m

211 Dundas Street East, Suite 202,
Belleville, Ontario, K8N 1E2

8.0 URBAN DESIGN, SUSTAINABILITY AND LAND USE ASSESSMENT

This section of the Planning Report assesses the land use compatibility, urban design and sustainability of the East Village subdivision in light of the applicable Cobourg Official Plan sustainability and design policies. The summary provided below sets out the direction and actions proposed to achieve a sustainable plan for the Phase 5 East Village project.

URBAN DESIGN + SUSTAINABILITY + LAND USE ASSESSMENT	
SUSTAINABILITY STRATEGY (OP Section 4.8.2)	
i)	<p><i>reduce the consumption of energy, land and other non-renewable resources including support for energy efficient building and opportunities for co-generation;</i></p> <p>The East Village – Phase 5 subdivision is within a designated settlement area, which shall be the focus of growth, and will not consist of consumption of rural, agricultural or natural heritage lands. Urban development of this nature reduces energy loss through delivery versus rural levels of service. East Village – Phase 5 proposes townhouses, 10-plexes and low-rise apartment buildings, which are generally considered to be progressively energy efficient housing types, respectively.</p>
ii)	<p><i>minimize the waste of materials, water and other limited resources;</i></p> <p>It is understood that every effort will be made during the construction phase of the project to minimize waste of materials, water and other limited resources. This policy may best be implemented however by including as a clause in the subdivision agreement.</p>
iii)	<p><i>create livable, healthy and productive environments; and,</i></p> <p>Parkland is provided within Phases 1 to 4 and proposed Phase 5, along with public sidewalks on both sides of each road. These features will facilitate a livable and healthy environment for current and future residents of the East Village development. The proposed storm water facility is to be located at the southerly entrance to Phase 5 on a prominent corner, with frontage on three roadways. This facility will facilitate a productive environment by providing quality and quality storm water management but also provide aesthetic value to the development as an amenity feature at the entrance to the subdivision – complete with a pathway around the</p>

URBAN DESIGN + SUSTAINABILITY + LAND USE ASSESSMENT

perimeter and landscaping around the top and side slopes of the pond. The gentle side slopes were designed in accordance with GRCA standards to prevent the need for fencing.

iv) reduce greenhouse gases.

It is understood that lands are developed or approved for employment and commercial uses immediately to the south and east of East Village – Phase 5. This will mitigate greenhouse gas emissions through a potential reduction of vehicle trips. Close proximity to the employment and commercial uses from Phase 5 will encourage current and future residents to take other, more active forms of transportation such as walking, cycling, etc. to these uses.

v) enhance biodiversity, ecological function, and the natural heritage system, including the provision of wildlife habitat and linkages.

- **There are no Greenland systems or linkages within or surrounding the East Village – Phase 5 subdivision. There are no watercourses on the property and the site is relatively flat with a slight northeastern to southwest slopes. The site is predominantly deciduous thicket with scattered trees (Black Locust, Cottonwood and Poplar) and shrubs. Street trees will be planted on the boulevard area within the road allowance to maintain and enhance the existing urban forest. Street trees, along with buffering and amenity areas will be planted according to a Landscape Plan prepared as part of the draft plan approval. An isolated, local wetland has been identified along the eastern property line. The proposed development will protect the wetland and identified Monarch habitat through the dedication of the road widening to the Town and a 30-metre setback. Low Impact Development (LID) features have been incorporated to promote infiltration of storm water, wherever possible. Downspouts from building rooftops will be directed to vegetated areas. A construction, erosion and sediment control plan will be prepared.**

URBAN DESIGN + SUSTAINABILITY + LAND USE ASSESSMENT
GENERAL DESIGN POLICIES (OP Section 5.2)
<ul style="list-style-type: none"> • Street System <ul style="list-style-type: none"> 5.2.1.1 <i>Street System</i> <ul style="list-style-type: none"> i) <i>Street patterns in newly developing areas will be designed to reflect an interconnected street system with a modified grid pattern or other approaches, which facilitate continuous and direct movement within a development area, and between the development area, abutting areas and the existing community. In addition, blocks should be limited in length to reduce the need for mid-block connections and promote active transportation.</i> <p>A modified grid pattern has been utilized for the Phase 5 road layout and throughout the entire East Village development, incorporating ‘T’ intersections where possible. Intersection spacing ranges from approximately 100 metres to 200 metres, which is considered an ideal separation distance to promote active transportation.</p> ii) <i>Street patterns in existing areas shall not be modified except:</i> <ul style="list-style-type: none"> a) <i>where the rights-of-way are significantly below Town standards and the need for widening has been identified in the 1992 Transportation Study or an update of that study, recognizing that in Heritage Conservation Districts generally widening’s shall not be permitted;</i> <p>Road widening’s have been provided along the East Village – Phase 5 frontages of King Street East and the Willmott road allowance along the eastern property line.</p> b) <i>to introduce traffic calming techniques;</i> <p>“T” intersections have been utilized within Phase 5 where possible as a means of traffic calming. Shorter intersection spacing ranging from approximately 100 metres to 200 metres are intended to also facilitate traffic calming. Along the longer rights-of-way for Drewery Road (within Phase 4) and Hayward Street within Phase 5, horizontal curves have been incorporated to also provide traffic calming.</p> c) <i>through the introduction of boulevards, wide sidewalks, landscaped medians or other techniques on arterial and collector roads to mitigate the visual impact of existing wide rights-of-way;</i>

URBAN DESIGN + SUSTAINABILITY + LAND USE ASSESSMENT

East Village – Phase 5 has frontage on to two arterial roadways – King Street East, an existing arterial; and a future arterial along the eastern property line. The widening's provided along King Street East and the Willmott arterial for future planning purposes will facilitate boulevards, wide sidewalks and landscaped medians and other visual mitigation techniques.

- d) the introduction of bicycle paths and lanes, as appropriate to the function of the road;*

East Village – Phase 5 consists entirely of local, 20-metre road allowances. In accordance with the Town of Cobourg Urban Design and Landscape Guidelines, local roadways do not afford bicycle paths or lanes as an appropriate function of these roads. King Street East, an existing arterial road, consists of bicycle lanes on east side of the road, which current and future residents may access directly from Phase 5.

- e) the introduction of additional streets to improve connectivity within the street system; and,*

East Village – Phase 5 proposes new municipal roadways as extensions of the existing, established road network surrounding the development – Drewery Road, Hayward Street and Maplewood Boulevard. This, combined with intersection spacing ranging from approximately 100 metres to 200 metres will maximize connectivity of the proposed development.

- f) other modifications deemed necessary by the Town to improve the operation of the street.*

East Village – Phase 5 has been prepared in consultation with Town staff; however, modifications will be considered through the subdivision review process to create the best plan possible.

URBAN DESIGN + SUSTAINABILITY + LAND USE ASSESSMENT

- Streetscape Design

- 5.2.1.2 Streetscape Design

- i) *there is no reverse lotting;*

- East Village – Phase 5 does not propose reverse lotting.**

- ii) *there is unobstructed street frontage adjacent to public open space and Environmental Constraint Areas, where appropriate, and subject to appropriate design to ensure that there will be no negative environmental impacts;*

- Proposed subdivision features that will become public facilities, such as the parkland and storm water facility will each have direct, unobstructed street frontage on three roadways. There are no environmental constraint areas within East Village – Phase 5 and there are no apparent environmental impact concerns.**

- iii) *pavement, sidewalk and right-of-way widths, as well as the provision of bicycle lanes and paths, reflect the function of the street;*

- 20-metre road allowances have been provided within East Village – Phase 5 and which will afford sidewalks on both sides of the road and an 8.5-metre wide road surface. All road allowances proposed in Phase 5 are local where bicycle lanes and paths do not reflect the function of these streets and have not been provided.**

- iv) *the siting of parking areas, and the siting and massing of buildings and structures on all streets, but particularly on arterial and collector streets, shall generally assist in the creation of high quality streetscapes and, in particular, regard shall be had to providing a sense of enclosure to the street by situating building mass adjacent to the street, and particularly at intersections, to frame the street space;*

- This policy is particularly relevant when assessing the proposed medium- and high-density blocks for Phase 5. Figure 3 – Development Site Plan demonstrates potential layouts for the 10-plex buildings and apartment buildings. These blocks have sufficient frontage and depth to facilitate parking areas in the side yards or rear yards. This will allow for minimal setback of the buildings to the public roads and maximize the aesthetic value of the streetscapes on which they are located.**

URBAN DESIGN + SUSTAINABILITY + LAND USE ASSESSMENT

v) in residential areas, garages shall be designed so that they are not the dominant feature in the streetscape. In particular, garages shall not:

a) project beyond the facade of the residence; and,

Section 2.2 of this report provides an overview of the site features for the built portions of Phases 1 and 2 of the East Village subdivision. Stalwood Homes is the builder for both Phases 1 and 2 and the proposed Phase 5, and the same housing products are anticipated for the current draft plan. Section 2.2 demonstrates that all garages do not project beyond the façade of residences.

b) dominate the frontage of the lot, unless plans are submitted by the applicant to demonstrate to the satisfaction of the Town that the garages can be appropriately integrated with the streetscape;

The proposed housing products feature single and double garages; however, these are proportional based on the frontage of the building and do not appear to be dominant on the lot. Garage door colours are lighter or similar colours to the brick façade, which has the effect of diminishing its presence and promote integration with the streetscape.

vi) parking areas for non-residential uses or apartments or other large scale residential uses shall be designed to minimize areas where they directly front on the street, and where they do front on the street to reduce their visual impact both on the adjoining streetscape and on users by:

a) screening of the lot at the street through the use of such features as low fences, walls and a substantial landscaping buffer, excluding the area of any buildings and driveway crossings;

Building and parking area placement shown on the Development Site Plan for the 10-plex and apartment buildings is conceptual and subject to site plan approval. The Plan demonstrates however, that areas can be provided to include a screening buffer of these lots through hard and soft landscaping features to reduce their

URBAN DESIGN + SUSTAINABILITY + LAND USE ASSESSMENT

visual impact both on the adjoining streetscape and on users.

- b) locating the building and parking on the site in a manner which reduces their impact on the street, and where buildings are located close to the streetline, no parking shall be permitted between the buildings and the street;*

The Development Site Plan has sited the 10-plex and apartment buildings close to the street line and parking areas in the interior or rear yards of the blocks on which they are located. No parking has been provided between the buildings and the street.

- c) a reduction in the scale of large parking areas through their subdivision into smaller areas by means of landscaping; and,*

The Development Site Plan demonstrates that the lot areas for the 10-plex and apartment blocks are of sufficient size to allow for smaller parking areas to be created by means of landscaping. Adjustments can be made at the time of site plan approval for these blocks to provide sufficient landscaping to the satisfaction of the Town.

- d) joint access where feasible;*

The proposed 10-plex and apartment blocks are intended to consist of two or more buildings with joint access. There is an exception for the one 10-plex block, where only one building and access are contemplated.

- vii) landscaping and/or architectural features provides for features such as the definition of the street and public open space, framing of views and focal points, direction of pedestrian movement and demarcation of areas with different functions as appropriate through features such as low masonry walls/ metal fences, rockery, special building and roofline treatments, landmark features (e.g. clocks, towers, cupolas, bays, pergolas, weather vanes, art), outdoor activity areas (e.g. patios, plazas, squares), extensive plant material and other identifiable features; and;*

The single-detached, semi-detached and townhouse dwellings will not be subject to site plan approval and residents may landscape their freehold lots at their discretion. The blocks for

URBAN DESIGN + SUSTAINABILITY + LAND USE ASSESSMENT

the 10-plex and apartment buildings shall be subject to site plan approval and detailed landscape plans will be prepared at this stage to the satisfaction of the Town. Definition of the street is intended to largely be by way of principal façade of the dwellings and street trees. More extensive plantings are anticipated for the 10-plex and apartment blocks.

viii) service and open storage areas and utilities and services should be sited away from prominent views from the street to minimize impacts, alternative design options for locating such facilities should be considered, and only where there is no alternative should they be sited in view sensitive locations, in which case decorative screening shall be required.

All services and utilities are proposed to be located underground and there are no apparent impacts on prominent views or streetscape. There are no open storage uses associated with residential development.

- Views

5.2.2 Views

- i) New development shall be designed to preserve, enhance and/or create significant views of the following features:*
 - a. Lake Ontario;*
 - b. view from the Harbour to the Victoria Hall Clock Tower in accordance with the requirements of the Harbour Area Secondary Plan;*
 - c. important institutional or other buildings;*
 - d. open spaces; and,*
 - e. natural features.*

The extensive unobstructed frontages provided for the proposed parkland and storm water facility will create a sense of place and provide multiple vantage points and breaks in the streetscape to assist in wayfinding through the development. The storm water facility shall be landscaped and designed to not require fencing, providing a significant feature and “window” into East Village – Phase 5.

- ii) Public buildings and other major buildings and structures shall be encouraged to locate:*

URBAN DESIGN + SUSTAINABILITY + LAND USE ASSESSMENT

- a. *at the termination of a street or view corridor; and,*
- b. *at the intersections of arterial or collector roads.*

Blocks 60 and 61 are proposed as larger higher density parcels consisting of 142 units. These block are located at the terminus of Hayward Street. The Development Site Plan demonstrates that views to the east can be maintained from the terminus of Hayward Street. Access to these blocks from King Street East, an existing arterial, will be via a series of short roadways and will pass through a minimal portion of the development. Primary access for the apartment block will be from King Street East.

- **Landscape Design**

- 5.2.4 *Landscape Design*

- The Town shall ensure that appropriate landscaping is provided in new development and redevelopment to:*

- i) *maintain and enhance the character of existing developed areas; and,*

- East Village – Phase 5 provides for a transition of density and housing types. In particular, Lots 1 to 10 propose single-detached dwellings on lots with similar frontage and depth as the existing single-detached dwelling located off King Street East. This will afford similar private amenity areas in the rear yard as the existing dwellings and maintain the character in the immediate area.**

- ii) *allow for the creation of strong landscaped features in newly developing areas, including stormwater management facilities which shall be designed as important components of the Greenlands System, such that they will be incorporated in development in a manner which enhances views and access through street frontage; and,*

- The proposed storm water facility will act as a strong landscape feature at the entrance to the subdivision complete with a pathway around the perimeter and landscaping around the top and side slopes of the pond to enhance views and access through the street frontages. Gentle side slopes were designed in accordance with GRCA standards to prevent the**

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need for fencing. A landscaping plan will be developed for the pond and surrounding area during the detailed design stage.

- iii) *protect and enhance environmental features and open space areas having regard for the policies of Section 4, Greenlands System.*

The parkland dedicated from Phases 1 to 4 and the parkland proposed with Phase 5 of the East Village subdivision has been provided in accordance with the Planning Act; however, this open space feature is not linked environmentally with the Greenlands System. As part of the overall storm water management strategy however, an overland drainage flow corridor is proposed to protect existing drainage patterns.

- **Safe Community Design**

5.2.5 Safe Community Design

To promote safety and security in public places and minimize the potential for crime urban design should result in clear, unobstructed views of areas such as parking lots, parks, public institutions and open spaces, while streetscapes should be designed to encourage active public use and natural surveillance opportunities. In particular, the following measures shall be followed:

- i) *the design and siting of new buildings and structures shall provide opportunities for visual overlook and ease of public access to adjacent streets, parks and open spaces;*

Lots have been laid out surrounding the proposed parkland to front on and overlook the park. Housing designs have maximized the windows along the front façade to promote natural surveillance opportunities.

- ii) *clear, unobstructed views to parks and open spaces shall be provided from adjoining streets;*

The proposed parkland features frontage on three adjoining streets, which will afford unobstructed views. All four sides of the proposed parkland will have buildings overlooking the park.

URBAN DESIGN + SUSTAINABILITY + LAND USE ASSESSMENT
<p>iii) <i>appropriate lighting, visibility and opportunities for informal surveillance shall be provided for all walkways, parking lots, parking garages and open space areas;</i> Street lighting will be provided to the satisfaction of the Town, including the perimeter pathway around the storm water facility. Lighting plans for the 10-plex and apartment blocks will be determined through site plan approval to the satisfaction of the Town.</p> <p>iv) <i>landscape elements shall be selected and sited in order to maintain views for safety and surveillance;</i> Landscaping of the lots of single-detached, semi-detached and townhouse dwellings are at the discretion of the individual owner. Landscaping however, of the 10-plex and apartment blocks will be designed to the satisfaction of the Town to ensure views for safety and surveillance are maintained.</p> <p>v) <i>the sharing of such facilities as parking and walkways shall be encouraged to increase use and public presence in such areas;</i> The Development Site Plan demonstrates potential shared/common walkways and parking areas.</p>
<ul style="list-style-type: none"> • Accessibility and Visitability 5.2.6 <i>Accessibility and Visitability</i> <i>It shall be a policy of the Town to work towards the goal of equal access for all persons to public buildings and facilities, and to encourage the public and private sector to adopt similar goals with respect to their facilities as required to implement the Ontarians with Disabilities Act. The Town shall work with the Cobourg Accessibility Committee and citizens to continually refine and update a comprehensive, long term strategy for implementation which identifies barriers and proposes options to overcome them, allocates resources for education and change and monitors progress towards those ends. In particular, the principles of universal design should be applied to all public spaces and within new developments to ensure access and visitability for all individuals, while recognizing the need for balance where designated heritage properties or properties in Heritage Conservation Districts are involved. Regard shall also be given to the Ontario Building Code and the Ontarians with Disabilities Act, as well as the Town's Urban and Landscape Design Guidelines, the Heritage District Guidelines and, where appropriate, the Parks Canada Standards and Guidelines for the Conservation of Historic Places in Canada.</i>

URBAN DESIGN + SUSTAINABILITY + LAND USE ASSESSMENT
<p>All sidewalks will be barrier-free. Details of the sidewalk will be confirmed during detailed design stage in accordance with all current accessibility regulations, including the Ontarians with Disabilities Act and Ontario Building Code. East Village – Phase 5 includes apartment-style dwelling units. Elevators will service the apartment dwellings, where 100% of the units will have barrier-free access.</p>
<ul style="list-style-type: none"> • Active Transportation <i>5.2.7 Active Transportation</i> <i>In considering the design of public and private facilities, a key consideration shall be features, which contribute to enhancements to the ability for movement by pedestrians and bicyclists and other active transportation modes, as well as individuals with mobility challenges while recognizing the need for balance in Heritage Conservation Districts. These include additions to the Town's trail system, the use of wide sidewalks, and the provision of bicycle parking and other facilities for bicycles. In particular, in considering pedestrian movement on specific sites, the following shall apply:</i> <ul style="list-style-type: none"> i) <i>direct and clearly defined pedestrian connections which form an extension of the public transportation system shall be required throughout sites with one main pedestrian connection between all buildings and all adjacent municipal streets and sidewalks, and secondary connections for improved pedestrian access as appropriate;</i> The Development Site Plan demonstrates extensive public sidewalks, which are proposed on both sides of each municipal road allowance. The Plan also demonstrates conceptually how the development located in the private realm – primarily the 10-plex and apartment buildings – may provide direct and clearly defined pedestrian connections to the public sidewalk network. ii) <i>principal pedestrian routes shall be functionally separated from parking and driveway areas and should utilize a variety of surface materials, textures, colour, landscaping and changes in elevation;</i> As the proposed development fronts onto the portion of King Street East, an arterial road, textured paving and other multi-sensory elements can be considered for the King Street crosswalk area in accordance with the Town's Urban Design

URBAN DESIGN + SUSTAINABILITY + LAND USE ASSESSMENT

and Landscape Guidelines. Further consultation with Town staff is required.

iii) on large sites street furniture such as benches, waste receptacles, bicycle racks and pedestrian level lighting should be provided along main pedestrian routes where appropriate; and,

It is understood that no changes to Kings Street East are planned by the Town at this time. All internal streets within East Village – Phase 5 are local roads and do not qualify as main pedestrian routes.

iv) opportunities for pedestrian connections with adjacent sites should also be explored, and will be required wherever feasible.

All proposed municipal roads within East Village – Phase 5 are extensions of existing streets and will facilitate the logical extension of the pedestrian network the same. Private pedestrian walkways will connect directly to the public sidewalk network. Potential pedestrian connections between the 10-plex and apartment blocks may be explored at the site plan approval stage.

- **External Building Design**

5.2.8 External Building Design

When considering, as part of site plan approval, the external design of buildings and the design of the adjacent streetscape, the Town shall take into account all the policies of this Plan, particularly the policies of this Community Design Strategy. In addition, in evaluating the external design of buildings the Town shall seek design which reflects the directions in the Town's Urban Design and Landscape Guidelines and, in particular:

i) includes sustainable building practices such as the use of green roofs and solar panels with the achievement of LEED or Energy Star or other similar certification or equivalent being encouraged;

The homes in the East Village Phase 5 Subdivision will have the following energy efficiencies and features:

- **All dwellings to have masonry fronts and high quality siding on balance, corner lots to be all masonry**
- **High efficiency gas furnaces c/w digital thermostat**
- **R-60 blown-in insulation in attic**

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- R-20 batts plus r5 foam sheeting r25 above grade walls
- R20 CI basement insulation
- Qualified insulation in exterior studded walls above grade
- Integrated Heat Recovery Ventilation (HRV) system
- Double-pane insulated windows with low E
- Optimal value engineered wood framing – floor joists bonded to 5/8 osb sub-floors with glue and screws
- Quality pre-engineered roof trusses
- Quality 3/8 plywood roof sheathing
- Engineered 2x6in. exterior and 2x4 interior wall construction
- Water efficient faucets and shower heads
- Low consumption (4.8 litres/flush) toilets
- Energy Star qualified rental on demand gas hot water tank
- Each home energy modeled and blower-door tested by independent energy advisor

ii) complements the massing patterns, rhythms, character and context of adjacent existing development, while recognizing that built form evolves over time and that new buildings should not necessarily replicate existing buildings;

East Village – Phase 5 is adjacent to an existing built-up area, part of which is under construction. Stalwood Homes is the Builder for East Village and similar housing products will be utilized from Phases 1 and 2 for Phase 5 as well. This will ensure massing patterns, rhythms and character will be complimentary throughout the development. The site abuts existing single-detached dwellings in the south-west corner. Proposed Lots 1 to 10 mimic the frontage and area of the existing development and will also consist of single-detached dwellings with generous rear yards. That said, the Plan will created a complementary and gradual transition from the existing development to the higher density built forms toward the north and east.

iii) clearly defines public and private spaces;

The public sidewalks within the road allowance will clearly define public and private property.

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iv) is designed to incorporate the principles of universal design to ensure access and visitability for all individuals, while recognizing the need for balance where designated heritage properties and properties in Heritage Conservation Districts are involved;

All sidewalks will be barrier-free. Details of the sidewalk will be confirmed during detailed design stage in accordance with all current accessibility regulations, including the Ontarians with Disabilities Act and Ontario Building Code. Elevators will service the proposed apartment dwellings, where 100% of the units will have barrier-free access.

v) is in scale with surrounding development;

Similar housing products from East Village Phases 1 and 2 are being utilized for Phase 5 with the addition of a block for apartment dwellings. That said, the scale of Phase 5 mimics that of Phases 1 and 2. The proposed comprehensive Plan of Subdivision affords a gradual transition from existing development and low density to high density and scale and massing of built form. The apartment block meets principal locational criteria of being adjacent to public open space and frontage and access from an arterial road.

vi) is designed to ensure that all buildings, and, in particular, any commercial, mixed use or institutional buildings are designed to animate and create a positive interface with the street through:

a) the use of architecture and facade treatment (e.g. landmark materials to reflect the heritage of the community;

Section 2.2 of this report demonstrates the use of high quality masonry, brick and siding by the builder in varying colours.

b) front doors and generous real windows, or in some cases three dimensional display windows, on any major walls facing streets;

Fenestration of major walls of the single-detached, semi-detached, townhouse and 10-plex dwellings facing streets has been maximized based on the housing type.

URBAN DESIGN + SUSTAINABILITY + LAND USE ASSESSMENT
<p>c) <i>strong pedestrian connections to the street, while drive through uses, where permitted, are carefully positioned to minimize impacts on the street;</i> All buildings will provide direct pedestrian connections to the street.</p> <p>d) <i>the location of outdoor activity areas (e.g. patios), landscaping and other site design elements.</i> The Development Site Plan demonstrates that front yard setbacks have been minimized. It is intended that front yards and exterior side yards act as the “public face” for each dwelling. Private amenity areas are provided in the rear yards for each dwelling.</p>
<ul style="list-style-type: none"> • Signage 5.2.9 <i>Signage</i> <i>Provisions for signs within private development shall comply with the Town’s Sign By-law. In addition, the appearance of signs should reinforce the character of development through design and choice of colour, material and their placement at entrance areas and on building facades. Signs should be integrated into the site plan and building design to ensure coordination of design.</i> No permanent signage has been installed at the entrance to Phases 1 and 2 at the intersection of Brook Road and Stirling Way, although it is understood that this is pending.

9. PLANNING ACT, RSO 1990, SECTION 51 (24)

The Planning Act establishes criteria in Section 51(24) that the approval authority must consider in approving a draft plan of subdivision, among other matters, to the health, safety, convenience, accessibility for persons with disabilities and welfare of the present and future inhabitants of the municipality, and to the following:

- (a) *The effect of the proposed subdivision on matters of provincial interest, as referred to in Section 2 of the Act;*
 - The application for Approval of a Plan of Subdivision is consistent with the 2014 PPS and will address the applicable Provincial interests set out in Section 2 (a-q) of the Planning Act.
- (b) *Whether the proposed subdivision is premature or in the public interest;*
 - The proposed development represents the logical and compatible expansion of the existing built up area. Land is developed to the west and south of the subject property. Municipal servicing is available without the need for unjustified or uneconomical expansion.
- (c) *Whether the plan conforms to the Official Plan and adjacent plans of subdivision;*
 - The draft plan of subdivision conforms to the policies of the Growth Plan, the County of Northumberland Official Plan and Town of Cobourg Official Plan. The subdivision land use and road network have been designed to integrate into the subdivisions located to the west. An OPA will be processed for Block 61, the High Density block on the draft plan.
- (d) *The suitability of the land for the purposes for which it is to be subdivided;*
 - The subject property is generally flat and gradually sloping from north to south. The site appears to have been previously cleared and graded. The EIS identified a degraded drainage feature that appeared to convey surface water that collected along the northern property boundary to a culvert beneath King Street East. The intermittent channel was poorly defined with unstable banks. There are no physical hazards within the parcel fabric of the subdivision that would be detrimental to residential development.
- (e) *The number, width, location and proposed grades and elevations of highways and the adequacy of them, and the highways linking the highways in the proposed subdivision with the established highway system in the vicinity and the adequacy of them;*

- The plan includes new internal public streets which will provide access to the subdivision from King Street East. Drewery Road and Hayward Street provide access to the subdivision to North Brook Road. The traffic impact study concludes that there will be no negative impact on the existing intersections and King Street East from the subdivision.
- (f) *The dimensions and shapes of the proposed lots;*
- The dimensions of the proposed lots meet the minimum requirements of the Zoning By-Law. The lots are regular in shape and appropriate.
- (g) *The restrictions or proposed restrictions on the lands to be subdivided or adjoining lands;*
- The proposed plan will be subject to comprehensive conditions of draft approval and a Subdivision Agreement.
- (h) *Conservation of natural resources and flood control;*
- The EIS indicated that there are no wetlands on the site. A Stormwater Management Report has been prepared to ensure the proposed stormwater management design will provide the required storage and outlet control necessary to reduce post-development flows to predevelopment flows or less. The Plan proposes a stormwater facility at the entrance to the subdivision off King Street East. Runoff from minor and major storm events will be conveyed to the pond via storm sewer and the road right of way.
- (i) *The adequacy of utilities and municipal services;*
- Utilities and municipal services are available to the site and will be adequate to service the proposed density.
- (j) *The adequacy of school sites;*
- Kawartha Pine Ridge District School Board and the Peterborough, Victoria, Northumberland and Clarington Catholic District School Board will need to confirm that area schools can adequately serve the proposed development.
- (k) *The area of land, excluding highways, to be dedicated for public purposes;*
- The Draft Plan of Subdivision includes Block 62 for a Storm water facility and Block 63 for parkland use. The storm water facility is 0.75 ha in area and is located at the entrance to the subdivision at the northeast corner of the intersection of Maplewood Boulevard and King Street East. The parkland is an extension of the parkland that is to be deeded to the Town

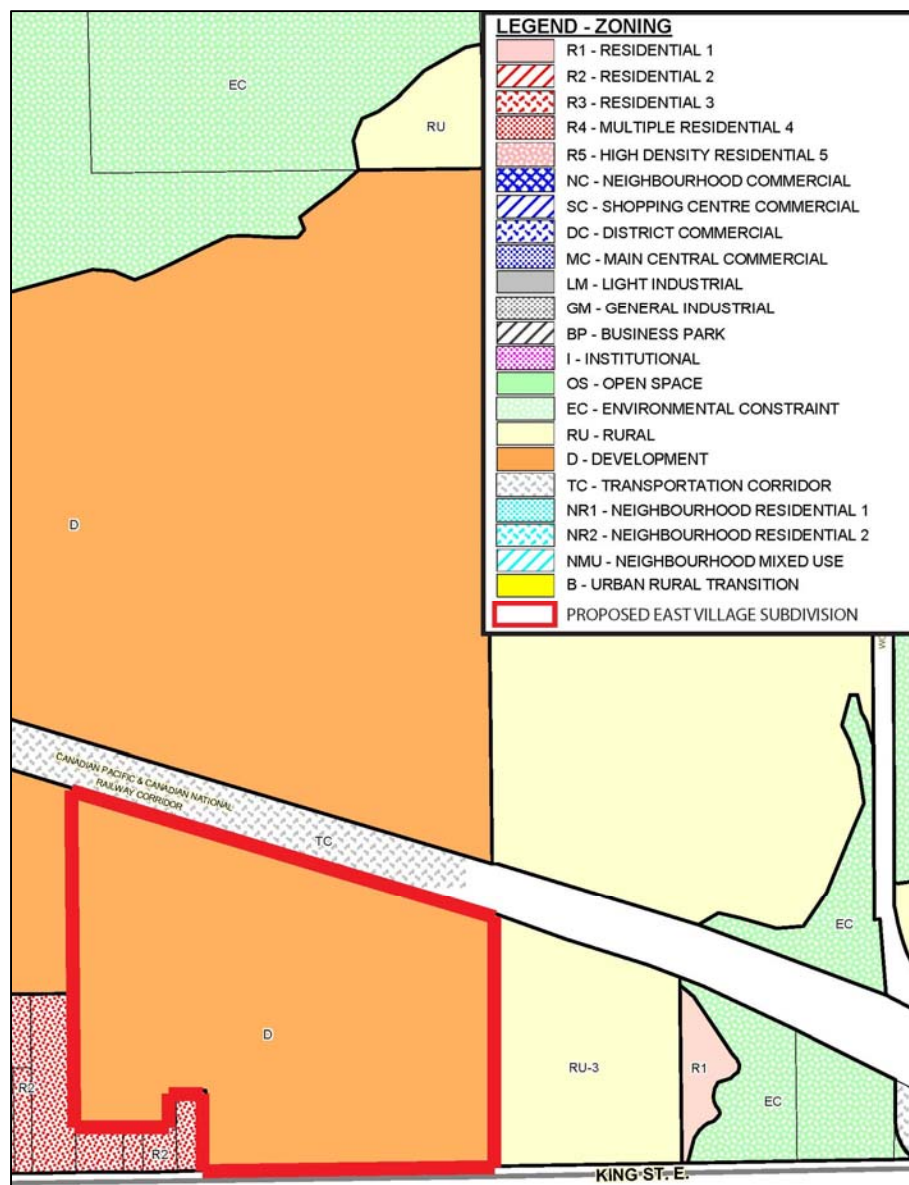
in an adjacent phase of the East Village subdivision. The size of the parkland dedication is .0.24 ha and the total area of the neighbourhood park will be 1.24ha. This is equivalent to 5% of the overall East Village neighbourhood.

- (l) *The extent to which the plan's design optimizes the available supply, efficient use and conservation of energy;*
- The plan includes a mix of housing including semi-detached dwellings, townhouses, 10-plexes and an apartment complex. The multiple unit dwellings can assist in energy conservation since there are energy savings associated with shared walls within the dwelling. Further, it is noted that the majority of lots have the ability to orient units such that they can maximize southern exposure. The road network is generally rectilinear which will allow for the efficient movement of traffic.
- (m) *The interrelationship between the design of the proposed plan of subdivision and site plan control matters relating to development on the lands, if the lands are located within a site plan control area designated under Section 41 of the Planning Act.*
- The plan of subdivision will be controlled by conditions of draft approval and a Subdivision Agreement approved by the Municipality, as well as site-specific zoning controls. Given that the townhouse units are freehold with no shared common facilities, site plan control will not be necessary. However, the 10-plexes and the apartment complex will be subject to site plan control.

10.0 ZONING BY-LAW 85-2003 ANALYSIS

The subject property is currently zoned OS and EC Zone on Map 13, By-law 85-2003. The relevant by-law extracts are found in **Appendix E**.

Figure 10: Extract of Existing Zoning



A rezoning of the subdivision lands is proposed (Refer to **Figure 11**). The parkland and stormwater blocks will be zoned OS Zone. The proposed layout of the draft plan has factored the applicable zoning regulations of the R4, R4-26, R4-29 and R5 Zones. All residential lots and blocks meet or exceed the various zone requirements.

This rezoning would be in accordance with the regulations for permitted uses found in the R4 and R5 Zones as reviewed in the following tables:

Table 2: R4-29 Zoning Analysis

ZONING BY-LAW No. 022-2018 - R4-29 ZONE						
DWELLING TYPE	SINGLE DETACHED DWELLING		SEMI-DETACHED DWELLING		TOWNHOUSE DWELLING PER UNIT	
ZONE PROVISION - SECTION 10	R4-29 ZONE	PROPOSED	R4-29 ZONE	PROPOSED	R4-28 ZONE	PROPOSED
MINIMUM LOT AREA	337.0m ²	393.7m ²	556.0m ²	601.4m ²	192.0m ²	215.2m ²
MINIMUM LOT FRONTAGE		12.0m	18.0m	19.4m	6.5m	7.15m
MINIMUM FRONT YARD	2.5 to 5.0m	5.0m	2.5 to 5.0m	5.0m	2.5 to 5.0m	5.0m
MINIMUM EXTERIOR SIDE YARD	3.0m	3.0m	3.0m	3.0m	3.0m	3.0m
MINIMUM INTERIOR SIDE YARD FOR 1 STOREY FOR EACH ADDITIONAL STOREY	0.75m 0.45m	1.2m	0.75m 0.45m	1.5m	0.75m 0.45m	1.2m
MINIMUM REAR YARD	7.0m	8.0m	7.0m	8.1m	7.0m	10.0m
MAXIMUM LOT COVERAGE	45.0%	41.5%	50.0%	44.1%	55.0%	46.0%
MINIMUM LANDSCAPED OPEN SPACE	30.0%	50.0%	30.0%	48.0%	30.0%	43.4%
MAXIMUM HEIGHT OF BUILDINGS	2 STOREY	2 STOREY	2 STOREY	2 STOREY	3 STOREY	3 STOREY
MAXIMUM No. OF DWELLINGS PER LOT	1	1	1	1	1	1

- The singles, semi-detached and townhouse lots and blocks will be rezoned to the R4-29 Zone. This is the same zone that has been formulated for Phases 1-4 of East Village. On Figure 3, typical building footprints for the various housing forms have been drawn on some of the smaller parcels in order to assess compliance with the various zone requirements. The lots and blocks will all meet or exceed the R4-29 Zone. It appears that there is no minimum lot frontage standard for the single detached dwelling in the R4-29 Zone.

Table 3: R4 Zoning Analysis

- Blocks 55 and 59 are proposed to be rezoned to the R4 Zone which permits a 3 storey apartment use. A 10 to 12 unit apartment has been

drawn on these blocks; they are not large enough for 2 10-plexes. Each block exceeds the R4 Zone requirements.

ZONING BY-LAW No. 85-2003 - R4 ZONE		
DWELLING TYPE	MULTI RESIDENTIAL (10 PLEX)	
ZONE PROVISION - SECTION 10	R4 ZONE	PROPOSED
MINIMUM LOT AREA	N/A	3003.0m ²
MINIMUM LOT FRONTAGE	30.0m	48.0m
MINIMUM FRONT YARD OR ESTABLISHED BUILDING LINE	6.0/3.55m	3.55m
MINIMUM EXTERIOR SIDE YARD	6.0m	15.0m
MINIMUM INTERIOR SIDE YARD	3.65m	3.65m
MINIMUM REAR YARD	7.0m	15.5m
MAXIMUM LOT COVERAGE	40.0%	11.5%
MINIMUM LANDSCAPED OPEN SPACE	35.0%	80.0%
MAXIMUM HEIGHT OF BUILDINGS	3 STOREYS	3 STOREYS
MAXIMUM No. OF BUILDINGS PER LOT	1	1
PARKING SPACES	1.5/UNIT	1.5/UNIT

Table 4: R4-26 Zoning Analysis

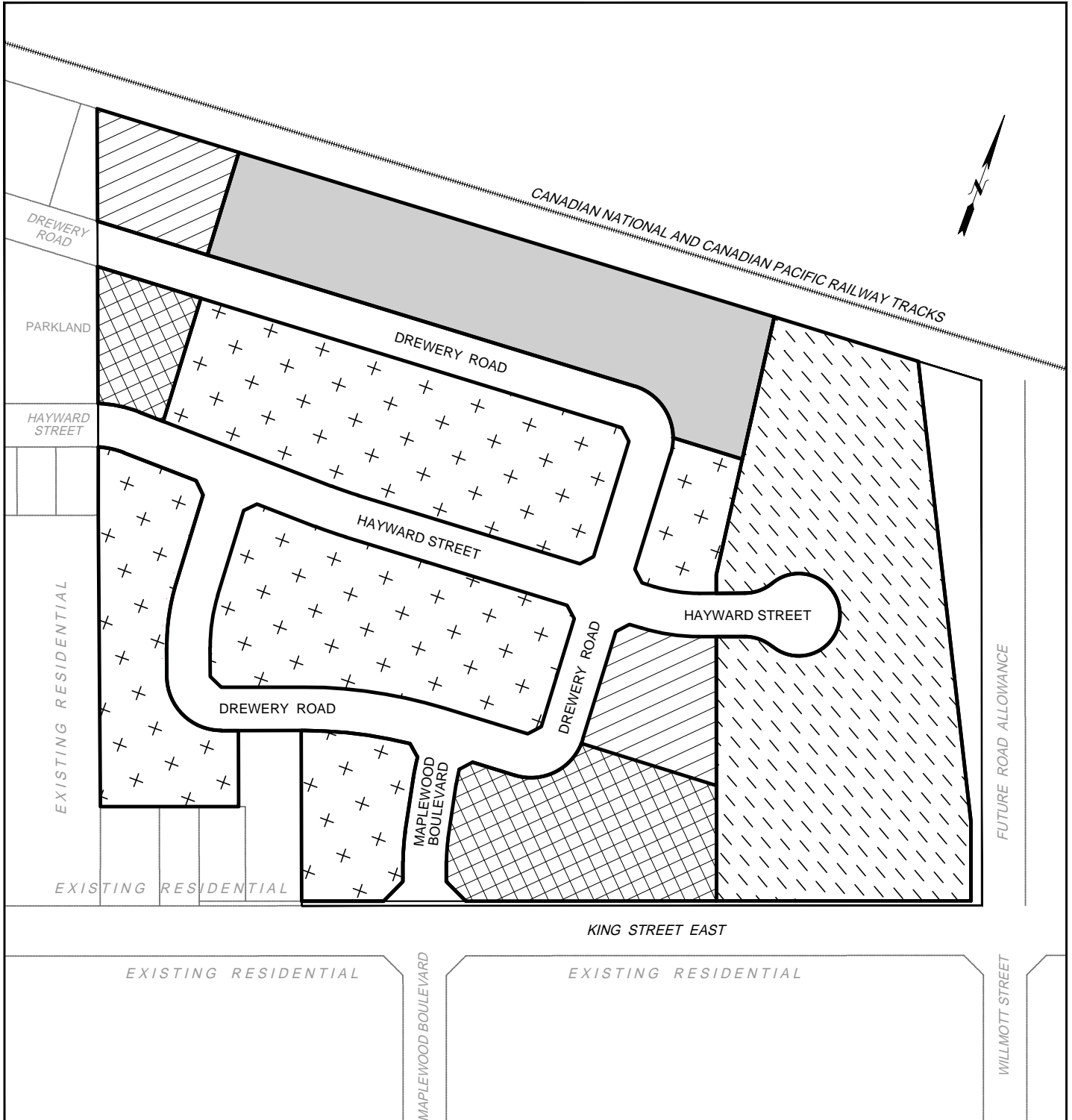
- Blocks 56-58 would be rezoned to the R4-26 Zone. This zone was established for Phases 1-4 of East Village. This is the zone category for the sites with the two 10-plex buildings. As shown on Figure 3, all of the blocks can be developed for the groupings of 2 10-plexes according to the R4-26 Zone.

ZONING BY-LAW No. 068-2012 - R4-26 ZONE		
DWELLING TYPE	IO PLEX DWELLING UNITS	
ZONE PROVISION - SECTION 10	R4-26 ZONE	PROPOSED
MINIMUM LOT AREA	N/A	4297.7m ²
MINIMUM LOT FRONTAGE	75.0m	87.0m
MINIMUM FRONT YARD	3.0m	3.55m
MINIMUM EXTERIOR SIDE YARD	6.0m	N/A
MINIMUM INTERIOR SIDE YARD	3.65m	3.65m
MINIMUM REAR YARD	7.0m	32.7m
MAXIMUM LOT COVERAGE	40.0%	16.0%
MINIMUM LANDSCAPED OPEN SPACE	35.0%	57.0%
MAXIMUM HEIGHT OF BUILDINGS	3 STOREYS	3 STOREYS
MAXIMUM No. OF BUILDINGS PER LOT	2	2
PARKING SPACES	1.5/UNIT	1.5/UNIT






Table 5: R5 Zoning Analysis

- Blocks 60 and 61 would be rezoned to special R5 Zones.
- Block 60 would be within the medium density category but it would consist of 2-4 three (3) storey apartment buildings of approximately 40 units. This zone would also permit seniors housing.
- Block 61 would also be zoned to the R5 Zone to permit up to two (2) 5 storey apartment buildings and 102 units. The use of Block 61 for a retirement home or seniors' supportive housing would also be allowed.
- On Table 5 the R5 Zone criteria are compared to the potential site plan for Block 61; all R5 Zone requirements are satisfied.

ZONING BY-LAW No. 85-2003 - R5 ZONE		
DWELLING TYPE	APARTMENT BLOCK	
ZONE PROVISION - SECTION II	R5 ZONE	PROPOSED
MINIMUM LOT AREA	NA	10526.4m ²
MINIMUM LOT FRONTAGE	30.0m	110.8m
MINIMUM FRONT YARD	6.0m	7.5m
MINIMUM EXTERIOR SIDE YARD	7.5m	12.0m
MINIMUM INTERIOR SIDE YARD	7.5m	13.5m
MINIMUM REAR YARD	7.5m	7.5m
MAXIMUM LOT COVERAGE	40.0%	13.0%
MINIMUM LANDSCAPED OPEN SPACE	35.0%	55.5%
MAXIMUM HEIGHT OF BUILDINGS	6 STOREYS	5 STOREY
PARKING SPACES	1.5/UNIT	1.5/UNIT
NET DENSITY - 100 UNITS/HA	142 UNITS	102 UNITS



LEGEND:

 FROM D to R4 (MULTIPLE RESIDENTIAL ZONE)	 FROM D to R4-26 (SPECIAL MULTIPLE RESIDENTIAL ZONE)	 FROM D to OS (OPEN SPACE ZONE)
 FROM D to R4-29 (SPECIAL MULTIPLE RESIDENTIAL ZONE)	 FROM D to R5 (HIGH DENSITY RESIDENTIAL ZONE)	

LOCATION

EAST VILLAGE - PHASE 5
PART OF LOTS 11 & 12,
CONCESSION A
GEOGRAPHIC TOWNSHIP OF HAMILTON
TOWN OF COBOURG
COUNTY OF NORTHUMBERLAND

**TO AMEND ZONING BY-LAW #85-2003
SCHEDULE '1'**



SCALE: 1:3000 DATE: DECEMBER 20, 2018
0 10 25 50 75 100 125m

211 Dundas Street East, Suite 202,
Belleville, Ontario, K8N 1E2

11.0 PLANNING OPINION AND CONCLUSION

This Planning Report was prepared in support of an application by JMCD Holdings Inc. for draft plan approval of a subdivision, Official Plan Amendment and Rezoning in Cobourg.

The property is located at the eastern limit of the urban settlement area on the north side of King Street East and south of the CN/CP rail line. The site is currently vacant and is approximately 11.9 hectares.

The subject property will be developed for 333 residential lots/units. The proposed density is 32 units per net hectare. Access to the lots will be from new internal roads that will extend from the existing road network in Phase 4 of the subdivision. The proposed lot sizes and use are in keeping with the established zoning for Phase 1-4 of East Village and provide a full mix of housing types and densities.

A multi-disciplinary approach was followed by the consultant team in developing the draft plan. There has been careful consideration of land use planning criteria, density analysis, building envelopes, setbacks, buffers, parkland and stormwater when preparing the draft plan. The policies of the Town of Cobourg Official Plan that set out criteria to be considered when developing within the Residential Area have guided the design process for the project.

The proposed plan of subdivision has had regard to the relevant criteria of The Planning Act RSO 1990, Section 51 (24) that are to be considered when granting draft approval, namely:

- a) The proposed subdivision is consistent on matters of provincial interest;
- b) The proposed subdivision is in the public interest; it will facilitate the development of a long-vacant parcel as a logical phase of development within the urban area;
- c) The draft plan will conform to the Official Plan and the adjacent plans of subdivision;
- d) The lands will be suitable for residential homes and units as outlined in the supporting studies submitted with the application;
- e) The access to the subdivision from King Street East will be safe/adequate and will enhance neighbourhood connectivity;
- f) The dimensions and shapes of the proposed lots and blocks will satisfy the requirements of Zoning By-Law 85-2003, as amended;
- g) The draft plan of subdivision will be subject to conditions of draft approval and a Subdivision Agreement;
- h) The plan respects the requirements of the GRCA;

- i) There will be adequate utilities and municipal services available to service the subdivision;
- j) There are adequate existing school sites in the neighbourhood;
- k) The draft plan will result in the creation of new internal roads that will connect to planned roads within the adjoining subdivision and will be dedicated for municipal purposes;
- l) The dwelling units in the subdivision will be energy efficient and the design of the plan optimizes passive solar gain.

The applications for Approval of a Draft Plan of Subdivision by JMCD Holdings Inc. in Cobourg are consistent with the policies of the Provincial Policy Statement, the Growth Plan, the Northumberland County Official Plan and the Cobourg Official Plan; will comply with the provisions of Zoning By-law 85-2003, as amended, and represents good planning.

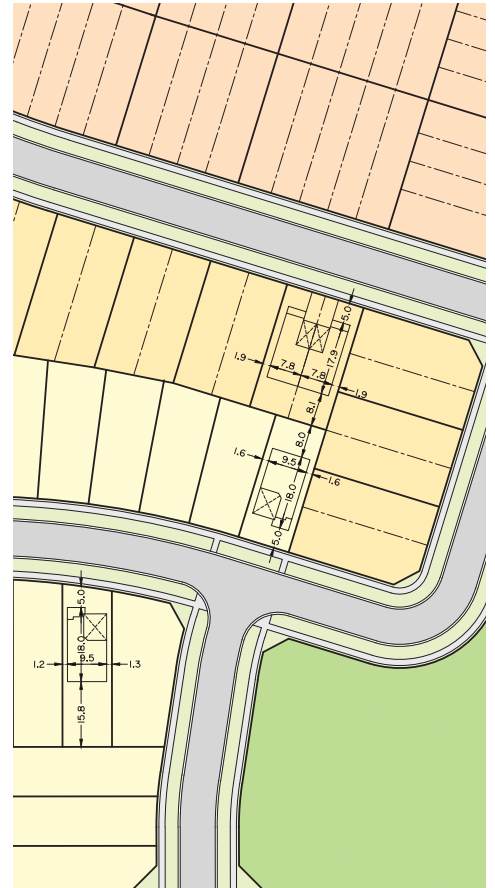
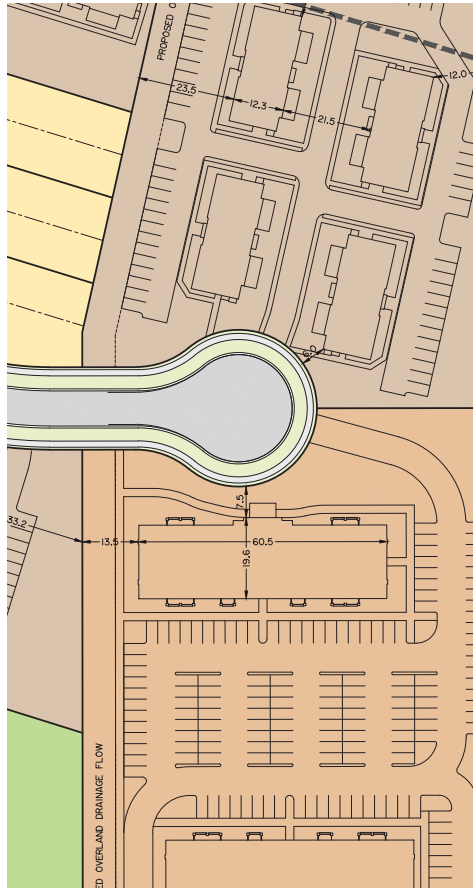
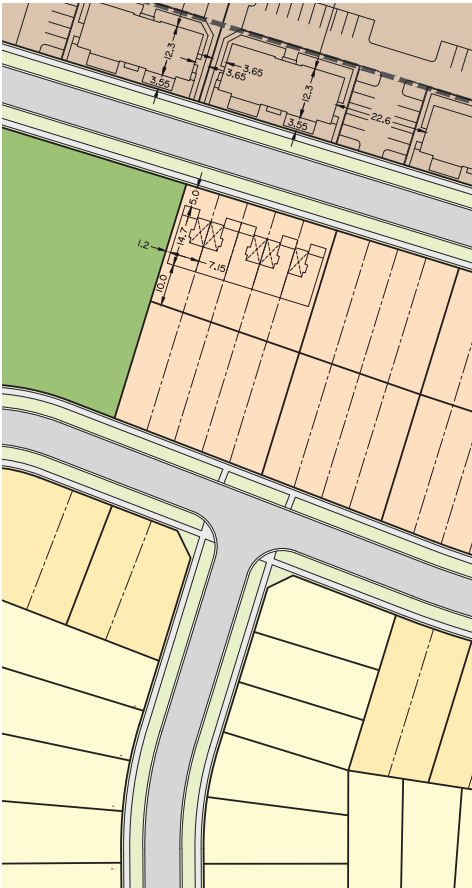
12.0 REPORT SIGNATURE

RFA PLANNING CONSULTANT INC.

Ruth Ferguson Aulhouse, MCIP, RPP President

APPENDIX 'A'

PROVINCIAL POLICY STATEMENT 2014 EXTRACTS



2014 Provincial Policy Statement

Under the *Planning Act*

Ontario.ca/PPS

- 1.1.2 Sufficient land shall be made available to accommodate an appropriate range and mix of land uses to meet projected needs for a time horizon of up to 20 years. However, where an alternate time period has been established for specific areas of the Province as a result of a provincial planning exercise or a *provincial plan*, that time frame may be used for municipalities within the area.

Within *settlement areas*, sufficient land shall be made available through *intensification* and *redevelopment* and, if necessary, *designated growth areas*.

Nothing in policy 1.1.2 limits the planning for *infrastructure* and *public service facilities* beyond a 20-year time horizon.

1.1.3 Settlement Areas

Settlement areas are urban areas and rural settlement areas, and include cities, towns, villages and hamlets. Ontario's settlement areas vary significantly in terms of size, density, population, economic activity, diversity and intensity of land uses, service levels, and types of infrastructure available.

The vitality of settlement areas is critical to the long-term economic prosperity of our communities. Development pressures and land use change will vary across Ontario. It is in the interest of all communities to use land and resources wisely, to promote efficient development patterns, protect resources, promote green spaces, ensure effective use of infrastructure and public service facilities and minimize unnecessary public expenditures.

- 1.1.3.1 *Settlement areas* shall be the focus of growth and development, and their vitality and regeneration shall be promoted.

- 1.1.3.2 Land use patterns within *settlement areas* shall be based on:

- a) densities and a mix of land uses which:
 - 1. efficiently use land and resources;
 - 2. are appropriate for, and efficiently use, the *infrastructure* and *public service facilities* which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;
 - 3. minimize negative impacts to air quality and climate change, and promote energy efficiency;
 - 4. support *active transportation*;
 - 5. are *transit-supportive*, where transit is planned, exists or may be developed; and
 - 6. are *freight-supportive*; and
- b) a range of uses and opportunities for *intensification* and *redevelopment* in accordance with the criteria in policy 1.1.3.3, where this can be accommodated.

- 1.1.3.3 Planning authorities shall identify appropriate locations and promote opportunities for *intensification* and *redevelopment* where this can be accommodated taking into account existing building stock or areas, including *brownfield sites*, and the availability of suitable existing or planned *infrastructure* and *public service facilities* required to accommodate projected needs.

Intensification and *redevelopment* shall be directed in accordance with the policies of Section 2: Wise Use and Management of Resources and Section 3: Protecting Public Health and Safety.

- 1.1.3.4 Appropriate development standards should be promoted which facilitate *intensification*, *redevelopment* and compact form, while avoiding or mitigating risks to public health and safety.
- 1.1.3.5 Planning authorities shall establish and implement minimum targets for *intensification* and *redevelopment* within built-up areas, based on local conditions. However, where provincial targets are established through *provincial plans*, the provincial target shall represent the minimum target for affected areas.
- 1.1.3.6 New development taking place in *designated growth areas* should occur adjacent to the existing built-up area and shall have a compact form, mix of uses and densities that allow for the efficient use of land, *infrastructure* and *public service facilities*.
- 1.1.3.7 Planning authorities shall establish and implement phasing policies to ensure:
- a) that specified targets for *intensification* and *redevelopment* are achieved prior to, or concurrent with, new development within *designated growth areas*; and
 - b) the orderly progression of development within *designated growth areas* and the timely provision of the *infrastructure* and *public service facilities* required to meet current and projected needs.
- 1.1.3.8 A planning authority may identify a *settlement area* or allow the expansion of a *settlement area* boundary only at the time of a *comprehensive review* and only where it has been demonstrated that:
- a) sufficient opportunities for growth are not available through *intensification*, *redevelopment* and *designated growth areas* to accommodate the projected needs over the identified planning horizon;
 - b) the *infrastructure* and *public service facilities* which are planned or available are suitable for the development over the long term, are financially viable over their life cycle, and protect public health and safety and the natural environment;

1.4 Housing

1.4.1 To provide for an appropriate range and mix of housing types and densities required to meet projected requirements of current and future residents of the *regional market area*, planning authorities shall:

- a) maintain at all times the ability to accommodate residential growth for a minimum of 10 years through *residential intensification* and *redevelopment* and, if necessary, lands which are *designated and available* for residential development; and
- b) maintain at all times where new development is to occur, land with servicing capacity sufficient to provide at least a three-year supply of residential units available through lands suitably zoned to facilitate *residential intensification* and *redevelopment*, and land in draft approved and registered plans.

1.4.2 Where planning is conducted by an upper-tier municipality:

- a) the land and unit supply maintained by the lower-tier municipality identified in policy 1.4.1 shall be based on and reflect the allocation of population and units by the upper-tier municipality; and
- b) the allocation of population and units by the upper-tier municipality shall be based on and reflect *provincial plans* where these exist.

1.4.3 Planning authorities shall provide for an appropriate range and mix of housing types and densities to meet projected requirements of current and future residents of the *regional market area* by:

- a) establishing and implementing minimum targets for the provision of housing which is *affordable to low and moderate income households*. However, where planning is conducted by an upper-tier municipality, the upper-tier municipality in consultation with the lower-tier municipalities may identify a higher target(s) which shall represent the minimum target(s) for these lower-tier municipalities;
- b) permitting and facilitating:
 - 1. all forms of housing required to meet the social, health and well-being requirements of current and future residents, including *special needs* requirements; and
 - 2. all forms of *residential intensification*, including second units, and *redevelopment* in accordance with policy 1.1.3.3;
- c) directing the development of new housing towards locations where appropriate levels of *infrastructure* and *public service facilities* are or will be available to support current and projected needs;
- d) promoting densities for new housing which efficiently use land, resources, *infrastructure* and *public service facilities*, and support the use

- of *active transportation* and transit in areas where it exists or is to be developed; and
- e) establishing development standards for *residential intensification*, *redevelopment* and new residential development which minimize the cost of housing and facilitate compact form, while maintaining appropriate levels of public health and safety.

1.5 Public Spaces, Recreation, Parks, Trails and Open Space

1.5.1 Healthy, active communities should be promoted by:

- a) planning public streets, spaces and facilities to be safe, meet the needs of pedestrians, foster social interaction and facilitate *active transportation* and community connectivity;
- b) planning and providing for a full range and equitable distribution of publicly-accessible built and natural settings for *recreation*, including facilities, parklands, public spaces, open space areas, trails and linkages, and, where practical, water-based resources;
- c) providing opportunities for public access to shorelines; and
- d) recognizing provincial parks, conservation reserves, and other protected areas, and minimizing negative impacts on these areas.

1.6 Infrastructure and Public Service Facilities

1.6.1 *Infrastructure*, electricity generation facilities and transmission and distribution systems, and *public service facilities* shall be provided in a coordinated, efficient and cost-effective manner that considers impacts from climate change while accommodating projected needs.

Planning for *infrastructure*, electricity generation facilities and transmission and distribution systems, and *public service facilities* shall be coordinated and integrated with land use planning so that they are:

- a) financially viable over their life cycle, which may be demonstrated through asset management planning; and
- b) available to meet current and projected needs.

1.6.2 Planning authorities should promote *green infrastructure* to complement *infrastructure*.

- 1.6.3 Before consideration is given to developing new *infrastructure* and *public service facilities*:
- a) the use of existing *infrastructure* and *public service facilities* should be optimized; and
 - b) opportunities for adaptive re-use should be considered, wherever feasible.
- 1.6.4 *Infrastructure* and *public service facilities* should be strategically located to support the effective and efficient delivery of emergency management services.
- 1.6.5 *Public service facilities* should be co-located in community hubs, where appropriate, to promote cost-effectiveness and facilitate service integration, access to transit and *active transportation*.

1.6.6 Sewage, Water and Stormwater

- 1.6.6.1 Planning for *sewage and water services* shall:
- a) direct and accommodate expected growth or development in a manner that promotes the efficient use and optimization of existing:
 - 1. *municipal sewage services* and *municipal water services*; and
 - 2. *private communal sewage services* and *private communal water services*, where *municipal sewage services* and *municipal water services* are not available;
 - b) ensure that these systems are provided in a manner that:
 - 1. can be sustained by the water resources upon which such services rely;
 - 2. is feasible, financially viable and complies with all regulatory requirements; and
 - 3. protects human health and the natural environment;
 - c) promote water conservation and water use efficiency;
 - d) integrate servicing and land use considerations at all stages of the planning process; and
 - e) be in accordance with the servicing hierarchy outlined through policies 1.6.6.2, 1.6.6.3, 1.6.6.4 and 1.6.6.5.
- 1.6.6.2 *Municipal sewage services* and *municipal water services* are the preferred form of servicing for *settlement areas*. *Intensification* and *redevelopment* within *settlement areas* on existing *municipal sewage services* and *municipal water services* should be promoted, wherever feasible.
- 1.6.6.3 Where *municipal sewage services* and *municipal water services* are not provided, municipalities may allow the use of *private communal sewage services* and *private communal water services*.

- 1.6.6.4 Where *municipal sewage services* and *municipal water services* or *private communal sewage services* and *private communal water services* are not provided, *individual on-site sewage services* and *individual on-site water services* may be used provided that site conditions are suitable for the long-term provision of such services with no *negative impacts*. In *settlement areas*, these services may only be used for infilling and minor rounding out of existing development.
- 1.6.6.5 *Partial services* shall only be permitted in the following circumstances:
- a) where they are necessary to address failed *individual on-site sewage services* and *individual on-site water services* in existing development; or
 - b) within *settlement areas*, to allow for infilling and minor rounding out of existing development on *partial services* provided that site conditions are suitable for the long-term provision of such services with no *negative impacts*.
- 1.6.6.6 Subject to the hierarchy of services provided in policies 1.6.6.2, 1.6.6.3, 1.6.6.4 and 1.6.6.5 planning authorities may allow lot creation only if there is confirmation of sufficient *reserve sewage system capacity* and *reserve water system capacity* within *municipal sewage services* and *municipal water services* or *private communal sewage services* and *private communal water services*. The determination of sufficient *reserve sewage system capacity* shall include treatment capacity for hauled sewage from *private communal sewage services* and *individual on-site sewage services*.
- 1.6.6.7 Planning for stormwater management shall:
- a) minimize, or, where possible, prevent increases in contaminant loads;
 - b) minimize changes in water balance and erosion;
 - c) not increase risks to human health and safety and property damage;
 - d) maximize the extent and function of vegetative and pervious surfaces; and
 - e) promote stormwater management best practices, including stormwater attenuation and re-use, and low impact development.

1.6.7 Transportation Systems

- 1.6.7.1 *Transportation systems* should be provided which are safe, energy efficient, facilitate the movement of people and goods, and are appropriate to address projected needs.
- 1.6.7.2 Efficient use shall be made of existing and planned *infrastructure*, including through the use of *transportation demand management* strategies, where feasible.

- b) optimizing the long-term availability and use of land, resources, *infrastructure*, electricity generation facilities and transmission and distribution systems, and *public service facilities*;
- c) maintaining and, where possible, enhancing the vitality and viability of downtowns and mainstreets;
- d) encouraging a sense of place, by promoting well-designed built form and cultural planning, and by conserving features that help define character, including *built heritage resources* and *cultural heritage landscapes*;
- e) promoting the redevelopment of *brownfield sites*;
- f) providing for an efficient, cost-effective, reliable *multimodal transportation system* that is integrated with adjacent systems and those of other jurisdictions, and is appropriate to address projected needs to support the movement of goods and people;
- g) providing opportunities for sustainable tourism development;
- h) providing opportunities to support local food, and promoting the sustainability of agri-food and agri-product businesses by protecting agricultural resources, and minimizing land use conflicts;
- i) promoting energy conservation and providing opportunities for development of *renewable energy systems* and *alternative energy systems*, including district energy;
- j) minimizing negative impacts from a changing climate and considering the ecological benefits provided by nature; and
- k) encouraging efficient and coordinated communications and telecommunications infrastructure.

1.8 Energy Conservation, Air Quality and Climate Change

1.8.1 Planning authorities shall support energy conservation and efficiency, improved air quality, reduced greenhouse gas emissions, and climate change adaptation through land use and development patterns which:

- a) promote compact form and a structure of nodes and corridors;
- b) promote the use of *active transportation* and transit in and between residential, employment (including commercial and industrial) and institutional uses and other areas;
- c) focus major employment, commercial and other travel-intensive land uses on sites which are well served by transit where this exists or is to be developed, or designing these to facilitate the establishment of transit in the future;
- d) focus freight-intensive land uses to areas well served by major highways, *airports*, *rail facilities* and *marine facilities*;
- e) improve the mix of employment and housing uses to shorten commute journeys and decrease transportation congestion;
- f) promote design and orientation which:

1. maximizes energy efficiency and conservation, and considers the mitigating effects of vegetation; and
 2. maximizes opportunities for the use of *renewable energy systems* and *alternative energy systems*; and
- g) maximize vegetation within *settlement areas*, where feasible.

2.0 Wise Use and Management of Resources

Ontario's long-term prosperity, environmental health, and social well-being depend on conserving biodiversity, protecting the health of the Great Lakes, and protecting natural heritage, water, agricultural, mineral and cultural heritage and archaeological resources for their economic, environmental and social benefits.

Accordingly:

2.1 Natural Heritage

- 2.1.1 Natural features and areas shall be protected for the long term.
- 2.1.2 The diversity and connectivity of natural features in an area, and the long-term *ecological function* and biodiversity of *natural heritage systems*, should be maintained, restored or, where possible, improved, recognizing linkages between and among *natural heritage features and areas, surface water features* and *ground water features*.
- 2.1.3 *Natural heritage systems* shall be identified in Ecoregions 6E & 7E¹, recognizing that *natural heritage systems* will vary in size and form in *settlement areas, rural areas, and prime agricultural areas*.
- 2.1.4 *Development and site alteration* shall not be permitted in:
 - a) *significant wetlands* in Ecoregions 5E, 6E and 7E¹; and
 - b) *significant coastal wetlands*.
- 2.1.5 *Development and site alteration* shall not be permitted in:
 - a) *significant wetlands* in the Canadian Shield north of Ecoregions 5E, 6E and 7E¹;
 - b) *significant woodlands* in Ecoregions 6E and 7E (excluding islands in Lake Huron and the St. Marys River)¹;
 - c) *significant valleylands* in Ecoregions 6E and 7E (excluding islands in Lake Huron and the St. Marys River)¹;
 - d) *significant wildlife habitat*;
 - e) *significant areas of natural and scientific interest*; and
 - f) *coastal wetlands* in Ecoregions 5E, 6E and 7E¹ that are not subject to policy 2.1.4(b)

unless it has been demonstrated that there will be no *negative impacts* on the natural features or their *ecological functions*.

¹ Ecoregions 5E, 6E and 7E are shown on Figure 1.

- 2.1.6 *Development and site alteration* shall not be permitted in *fish habitat* except in accordance with *provincial and federal requirements*.
- 2.1.7 *Development and site alteration* shall not be permitted in *habitat of endangered species and threatened species*, except in accordance with *provincial and federal requirements*.
- 2.1.8 *Development and site alteration* shall not be permitted on *adjacent lands* to the *natural heritage features and areas* identified in policies 2.1.4, 2.1.5, and 2.1.6 unless the *ecological function* of the *adjacent lands* has been evaluated and it has been demonstrated that there will be no *negative impacts* on the natural features or on their *ecological functions*.
- 2.1.9 Nothing in policy 2.1 is intended to limit the ability of *agricultural uses* to continue.

2.2 Water

- 2.2.1 Planning authorities shall protect, improve or restore the *quality and quantity of water* by:
- a) using the *watershed* as the ecologically meaningful scale for integrated and long-term planning, which can be a foundation for considering cumulative impacts of development;
 - b) minimizing potential *negative impacts*, including cross-jurisdictional and cross-*watershed* impacts;
 - c) identifying water resource systems consisting of *ground water features, hydrologic functions, natural heritage features and areas, and surface water features* including shoreline areas, which are necessary for the ecological and hydrological integrity of the *watershed*;
 - d) maintaining linkages and related functions among *ground water features, hydrologic functions, natural heritage features and areas, and surface water features* including shoreline areas;
 - e) implementing necessary restrictions on *development and site alteration* to:
 - 1. protect all municipal drinking water supplies and *designated vulnerable areas*; and
 - 2. protect, improve or restore *vulnerable* surface and ground water, *sensitive surface water features* and *sensitive ground water features*, and their *hydrologic functions*;
 - f) planning for efficient and sustainable use of water resources, through practices for water conservation and sustaining water quality;
 - g) ensuring consideration of environmental lake capacity, where applicable; and

- h) ensuring stormwater management practices minimize stormwater volumes and contaminant loads, and maintain or increase the extent of vegetative and pervious surfaces.

2.2.2 *Development and site alteration* shall be restricted in or near *sensitive surface water features* and *sensitive ground water features* such that these features and their related *hydrologic functions* will be protected, improved or restored.

Mitigative measures and/or alternative development approaches may be required in order to protect, improve or restore *sensitive surface water features*, *sensitive ground water features*, and their *hydrologic functions*.

2.3 Agriculture

2.3.1 *Prime agricultural areas* shall be protected for long-term use for agriculture.

Prime agricultural areas are areas where *prime agricultural lands* predominate. *Specialty crop areas* shall be given the highest priority for protection, followed by Canada Land Inventory Class 1, 2, and 3 lands, and any associated Class 4 through 7 lands within the *prime agricultural area*, in this order of priority.

2.3.2 Planning authorities shall designate *prime agricultural areas* and *specialty crop areas* in accordance with guidelines developed by the Province, as amended from time to time.

2.3.3 Permitted Uses

2.3.3.1 In *prime agricultural areas*, permitted uses and activities are: *agricultural uses*, *agriculture-related uses* and *on-farm diversified uses*.

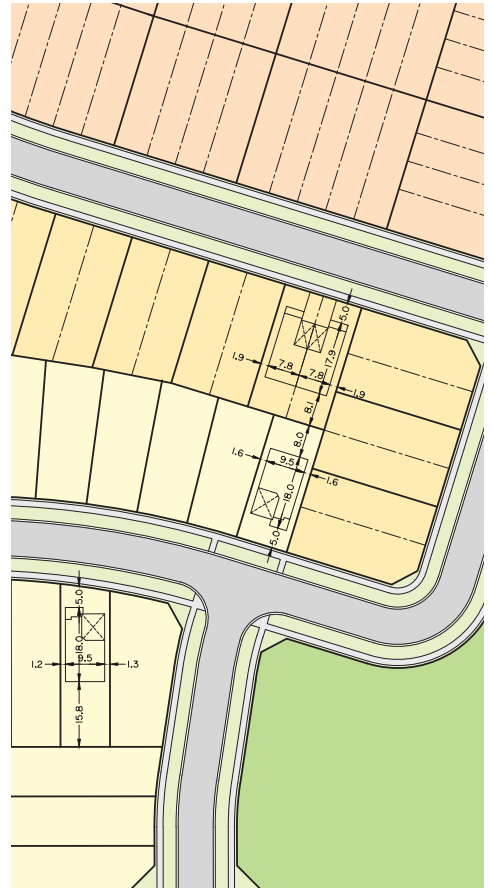
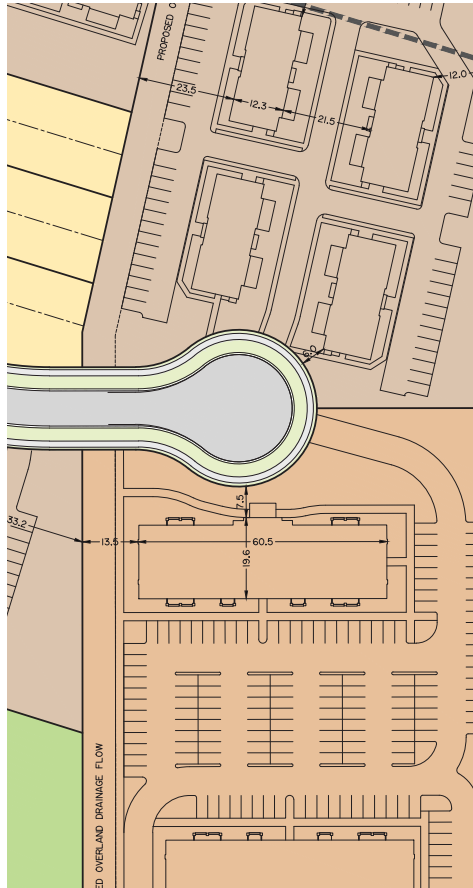
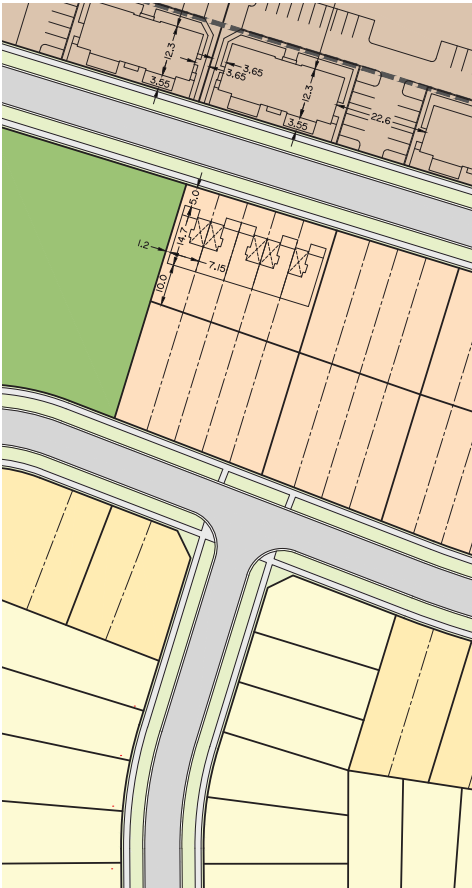
Proposed *agriculture-related uses* and *on-farm diversified uses* shall be compatible with, and shall not hinder, surrounding agricultural operations. Criteria for these uses may be based on guidelines developed by the Province or municipal approaches, as set out in municipal planning documents, which achieve the same objectives.

2.3.3.2 In *prime agricultural areas*, all types, sizes and intensities of *agricultural uses* and *normal farm practices* shall be promoted and protected in accordance with provincial standards.

2.3.3.3 New land uses, including the creation of lots, and new or expanding livestock facilities shall comply with the *minimum distance separation formulae*.

APPENDIX 'B'

GROWTH PLAN EXTRACTS



PLACES TO GROW

BETTER CHOICES. BRIGHTER FUTURE.



Growth Plan for the Greater Golden Horseshoe (2017)



2.2 Policies for Where and How to Grow

2.2.1 Managing Growth

1. Population and employment forecasts contained in Schedule 3 will be used for planning and managing growth in the *GGH* to the horizon of this Plan in accordance with the policies in subsection 5.2.4.
2. Forecasted growth to the horizon of this Plan will be allocated based on the following:
 - a) the vast majority of growth will be directed to *settlement areas* that:
 - i. have a *delineated built boundary*;
 - ii. have existing or planned *municipal water and wastewater systems*; and
 - iii. can support the achievement of *complete communities*;
 - b) growth will be limited in *settlement areas* that:
 - i. are *undelineated built-up areas*;
 - ii. are not serviced by existing or planned *municipal water and wastewater systems*; or
 - iii. are in the *Greenbelt Area*;
 - c) within *settlement areas*, growth will be focused in:
 - i. *delineated built-up areas*;
 - ii. *strategic growth areas*;
 - iii. locations with existing or planned transit, with a priority on *higher order transit* where it exists or is planned; and
 - iv. areas with existing or planned *public service facilities*;
 - d) development will be directed to *settlement areas*, except where the policies of this Plan permit otherwise;
 - e) development will be generally directed away from *hazardous lands*; and
 - f) the establishment of new *settlement areas* is prohibited.
3. Upper- and single-tier municipalities will undertake integrated planning to manage forecasted growth to the horizon of this Plan, which will:
 - a) establish a hierarchy of *settlement areas*, and of areas within *settlement areas*, in accordance with policy 2.2.1.2;
 - b) be supported by planning for *infrastructure* and *public service facilities* by considering the full life cycle costs of these assets and developing options to pay for these costs over the long-term;

- c) provide direction for an urban form that will optimize *infrastructure*, particularly along transit and transportation corridors, to support the achievement of *complete communities* through a more *compact built form*;
 - d) support the environmental and agricultural protection and conservation objectives of this Plan; and
 - e) be implemented through a *municipal comprehensive review* and, where applicable, include direction to lower-tier municipalities.
4. Applying the policies of this Plan will support the achievement of *complete communities* that:
- a) feature a diverse mix of land uses, including residential and employment uses, and convenient access to local stores, services, and *public service facilities*;
 - b) improve social equity and overall quality of life, including human health, for people of all ages, abilities, and incomes;
 - c) provide a diverse range and mix of housing options, including second units and *affordable* housing, to accommodate people at all stages of life, and to accommodate the needs of all household sizes and incomes;
 - d) expand convenient access to:
 - i. a range of transportation options, including options for the safe, comfortable and convenient use of *active transportation*;
 - ii. *public service facilities*, co-located and integrated in community hubs;
 - iii. an appropriate supply of safe, publicly-accessible open spaces, parks, trails, and other recreational facilities; and
 - iv. healthy, local, and affordable food options, including through urban agriculture;
 - e) ensure the development of high quality *compact built form*, an attractive and vibrant *public realm*, including public open spaces, through site design and urban design standards;
 - f) mitigate and adapt to climate change impacts, build resilience, reduce greenhouse gas emissions, and contribute towards the achievement of low-carbon communities; and
 - g) integrate *green infrastructure* and *low impact development*.
5. The Minister will establish a methodology for assessing land needs to implement this Plan, including relevant assumptions and other direction as required. This methodology will be used by upper- and single-tier

municipalities to assess the quantity of land required to accommodate forecasted growth to the horizon of this Plan.

6. Based on a land needs assessment undertaken in accordance with policy 2.2.1.5, some upper- and single-tier municipalities in the *outer ring* will determine that they have *excess lands*. These municipalities will:
 - a) determine which lands will be identified as *excess lands* based on the hierarchy of *settlement areas* established in accordance with policy 2.2.1.3; and
 - b) prohibit *development* on all *excess lands* to the horizon of this Plan.

2.2.2 Delineated Built-up Areas

1. By the year 2031, and for each year thereafter, a minimum of 60 per cent of all residential development occurring annually within each upper- or single-tier municipality will be within the *delineated built-up area*.
2. By the time the next *municipal comprehensive review* is approved and in effect, and each year until 2031, a minimum of 50 per cent of all residential development occurring annually within each upper- or single-tier municipality will be within the *delineated built-up area*.
3. Until the next *municipal comprehensive review* is approved and in effect, the annual minimum intensification target contained in the applicable upper- or single-tier official plan that is approved and in effect as of July 1, 2017 will continue to apply.
4. All municipalities will develop a strategy to achieve the minimum intensification target and *intensification* throughout *delineated built-up areas*, which will:
 - a) encourage *intensification* generally to achieve the desired urban structure;
 - b) identify the appropriate type and scale of development and transition of built form to adjacent areas;
 - c) identify *strategic growth areas* to support achievement of the intensification target and recognize them as a key focus for development;
 - d) ensure lands are zoned and development is designed in a manner that supports the achievement of *complete communities*;
 - e) prioritize planning and investment in *infrastructure* and *public service facilities* that will support *intensification*; and
 - f) be implemented through official plan policies and designations, updated zoning and other supporting documents.

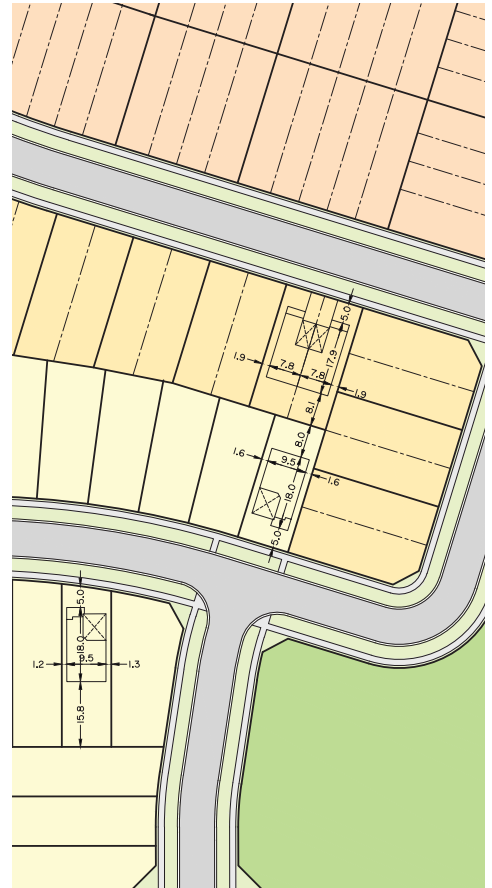
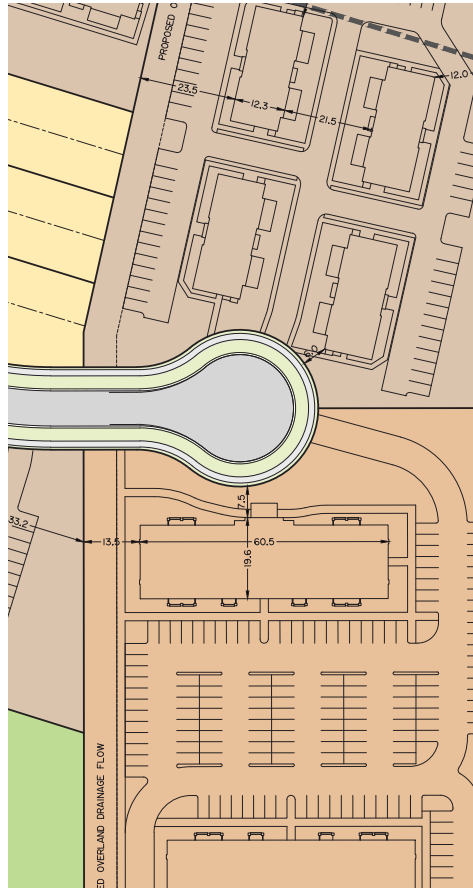
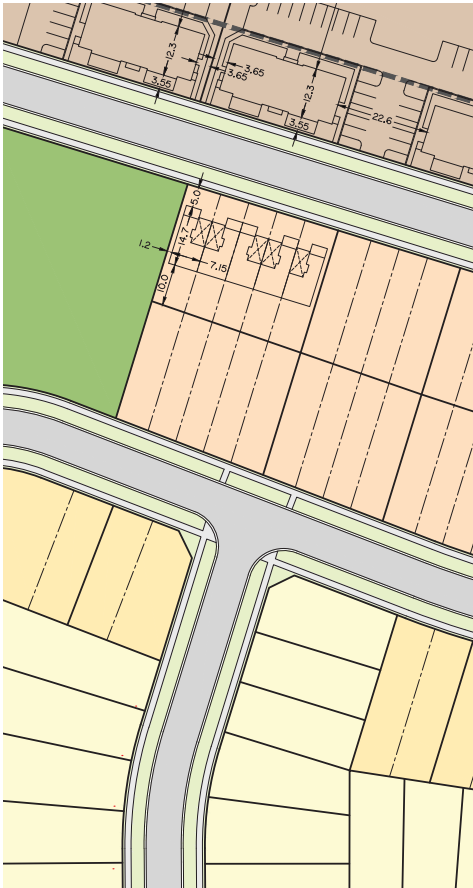
5. For upper- and single-tier municipalities, council may request an alternative to the target established in policy 2.2.2.2 through the next *municipal comprehensive review* where it is demonstrated that this target cannot be achieved and that the alternative target will:
 - a) maintain or improve on the minimum intensification target in the official plan that is approved and in effect;
 - b) be appropriate given the size of the *delineated built-up area*;
 - c) account for existing *infrastructure, public service facilities*, and capital planning;
 - d) account for existing planning approvals and other related planning studies;
 - e) consider the actual rate of *intensification* being achieved annually across the upper- or single-tier municipality;
 - f) support diversification of the total range and mix of housing options in *delineated built-up areas* to the horizon of this Plan, while considering anticipated demand;
 - g) account for lands where development is prohibited or severely restricted; and
 - h) support the achievement of *complete communities*.
6. For upper- and single-tier municipalities in the *outer ring*, council may request an alternative to the target established in policy 2.2.2.1 through a *municipal comprehensive review* where it is demonstrated that target cannot be achieved and that the alternative target is appropriate given the criteria in policy 2.2.2.5.
7. The Minister may permit an alternative to the target established in policies 2.2.2.1 and 2.2.2.2. If council does not make a request or the Minister does not permit an alternative target, the targets established in policies 2.2.2.1 and 2.2.2.2 will apply accordingly.

2.2.3 Urban Growth Centres

1. *Urban growth centres* will be planned:
 - a) as focal areas for investment in regional *public service facilities*, as well as commercial, recreational, cultural, and entertainment uses;
 - b) to accommodate and support the transit network at the regional scale and provide connection points for inter- and intra-regional transit;
 - c) to serve as high-density major employment centres that will attract provincially, nationally, or internationally significant employment uses; and

APPENDIX 'C'

COUNTY OF NORTHUMBERLAND OFFICIAL PLAN EXTRACTS





NORTHUMBERLAND COUNTY OFFICIAL PLAN

**As approved by the Ontario Municipal Board on
November 23, 2016**

B GROWTH MANAGEMENT

B1 URBAN AREAS/RURAL SETTLEMENT AREAS

Urban areas and rural settlement areas shall be the focus of growth and their vitality and regeneration shall be promoted.

B2 URBAN AREAS

A minimum of 80% of expected population and employment in the planning period is expected to occur in the six *urban areas* in the County as shown on **Tables A and B**. As the planning period for this Official Plan ends in 2034, the population and employment forecasts for 2034 apply. The forecasts for 2036 and 2041 shown on **Tables A and B** are included for planning purposes. However, these numbers extend beyond the planning period and cannot be relied upon to determine land needs beyond 2034.

TABLE A
Urban Area Population Growth Forecasts for 2034, 2036 and 2041

Urban Area	2011 - 2034 Population Growth Forecast	2011 - 2036 Population Growth Forecast	2011 - 2041 Population Growth Forecast	Share of Urban Areas Population Growth
Brighton	1,118	1,242	1,551	7.75%
Campbellford	1,065	1,183	1,478	7.38%
Cobourg	6,945	7,715	9,641	48.14%
Colborne	450	499	624	3.12%
Hastings	319	353	442	2.21%
Port Hope	4,531	5,033	6,290	31.41%
Total	14,426	16,026	20,026	

TABLE B
Urban Area Employment Growth Forecasts for 2034, 2036 and 2041

Urban Area	2011 - 2034 Employment Growth Forecast	2011 - 2036 Employment Growth Forecast	2011 - 2041 Employment Growth Forecast	Share of Urban Areas Employment Growth
Brighton	285	310	434	7.75%
Campbellford	272	295	413	7.38%
Cobourg	1,772	1,926	2,696	48.14%
Colborne	115	125	174	3.12%
Hastings	81	88	124	2.21%
Port Hope	1,156	1,256	1,759	31.41%
Total	3,680	4,000	5,600	

B6**POPULATION FORECAST FOR 2034 BY MUNICIPALITY**

Table G below establishes the population forecasts for 2034 by municipality.

TABLE G
POPULATION FORECAST FOR 2034 BY MUNICIPALITY

Municipality	2011 Population	2034 Population
Brighton	11,306	12,556
Trent Hills	13,040	14,606
Cobourg	19,160	26,105
Cramahe	6,283	7,013
Port Hope	16,775	22,145
Hamilton	11,072	12,359
Alnwick/Haldimand	6,846	7,733
Total	84,482	102,517

B7**HOUSING FORECAST**

Table H below establishes the housing forecast for each of the local municipalities. These forecasts are considered to be guidelines that are to be considered by each local municipality when their Official Plans are amended to conform with this Plan. In this regard, local municipalities may adjust the housing forecast and in particular the housing mix as appropriate taking into account the nature of existing planning approvals and the policies of the local Official Plan, provided it can be demonstrated that conformity with Sections B9 and B10 can be achieved.

TABLE H
HOUSING FORECASTS FOR 2034 BY MUNICIPALITY

Municipality	Low Density	Medium Density	High Density	Total
Brighton	392	205	74	671
Trent Hills	500	219	79	798
Cobourg	919	1,305	872	3,096
Cramahe	253	75	17	345
Port Hope	936	924	576	2,436
Hamilton	502	0	0	502
Alnwick/ Haldimand	337	0	0	337
Total	3,839	2,728	1,618	8,185

TABLE J
Minimum Greenfield Density Target for Greenfield Development in Urban Areas during the Planning Period

Municipality	Minimum Density Target for Greenfield Areas (people and jobs per gross hectare)
Brighton	25
Campbellford	25
Cobourg	35
Colborne	25
Hastings	25
Port Hope	35

The minimum Greenfield density target can be reviewed at the time of a County *municipal comprehensive review*.

B11 URBAN AREA AND RURAL SETTLEMENT AREA BOUNDARY EXPANSIONS

An expansion to an *urban area* or *rural settlement area* boundary may only occur as part of a County *municipal comprehensive review* as set out in Section 2.2.8 of the Growth Plan and as part of a *comprehensive review* as set out in Section 1.1.3.8 of the Provincial Policy Statement.

B12 EFFECT OF FORECASTS ON EXISTING PLANNING APPROVALS WITHIN URBAN AREAS AND RURAL SETTLEMENT AREAS

There is a sufficient supply of land designated in local Official Plans to meet the need for residential *development* over the 20 year time period of the Plan. However, the *urban area* population and employment forecasts set out in Tables A and B for *urban areas* and the population and employment forecasts for *rural lands* as set out in Tables C and D do not have an impact on the ability of the County and local municipalities to consider applications to develop lands that are within an *urban area* or *rural settlement area* boundary that existed on the date this Plan came into effect.

B13 PLANNING FOR EMPLOYMENT

- a) Ensuring that there is employment land in appropriate locations to attract the widest range of possible uses is a key objective of this Official Plan. On this basis, certain lands within the Municipality of Port Hope having an approximate area of 112 hectares shown on Schedule A have been identified as a preferred location for a future Major Employment Area designation. Until such time as the potential Port Hope Major Employment Area is approved in accordance with subsection B13 d) below, the designation of the subject lands shall remain as Agricultural. Other lands that are considered suitable for such

C LAND USE DESIGNATIONS

This Section of the Official Plan contains goals, objectives and policies with respect to the five land use designations on **Schedule A**. Additional policy direction is provided through local Official Plans. This section of the Official Plan also includes policies affecting lands that are subject to the Oak Ridges Moraine Conservation Plan (ORMCP).

C1 URBAN AREAS/RURAL SETTLEMENT AREAS

C1.1 COMPLETE COMMUNITIES

The County encourages each of the six *urban areas* to become *complete communities* where there is:

- a) A strong live/work ratio, where the majority of residents are employed in the community in which they live;
- b) Choice in the market place, in terms of obtaining goods and services;
- c) A range of educational and training opportunities;
- d) A range of housing types for all levels of income and ages;
- e) A range of health care services for the majority of the residents;
- f) A range of community and social services to assist the majority of those in need in the community;
- g) A range of cultural and recreational opportunities and facilities; and,
- h) A population level and density that supports the provision of public transit, where feasible.

For the purposes of this Plan, Warkworth is considered to be a *rural settlement area* where full municipal services are provided, and the policies of this Plan respecting *urban areas* in Section C of this Plan also apply to Warkworth.

C1.2 GENERAL LAND USE OBJECTIVES FOR URBAN AREAS

The following sections set out general land use objectives for residential, commercial, employment and open space areas within *urban areas* that are shown on **Schedule A** of this Plan.

C1.2.1 Residential Areas

It is the objective of this Plan to:

- a) Maintain and enhance the character and identity of existing residential areas;

- b) Encourage the provision of a range of housing types to accommodate persons with diverse social and economic backgrounds, needs and desires while promoting the maintenance and improvement of existing housing;
- c) Promote the efficient use of existing and planned *infrastructure* and *public service facilities* by supporting opportunities for various forms of *residential intensification*, where appropriate;
- d) Encourage increases in density in new *development* areas to maximize the use of *infrastructure* and minimize the amount of land required for new *development*;
- e) Promote a variety of complementary and compatible land uses in residential areas including *special needs* housing, community facilities, schools, small-scale commercial uses and recreational open space areas;
- f) Encourage a high standard of urban design for *development* and *redevelopment*;
- g) Encourage local municipalities to establish comprehensive design guidelines and policies to foster the establishment of communities that are safe, functional and attractive; and,
- h) Implement street designs that provide for pedestrian, cycling and other non-motorized modes of transportation to help create more healthy and complete communities.

C1.2.2 Commercial Areas

It is the objective of this Plan to:

- a) Encourage commercial *development* that will provide a full range of goods and services, at appropriate locations, to meet the needs of the County's residents, employees and businesses;
- b) Encourage and promote *development* that combines commercial, residential and other land uses to facilitate the more efficient use of urban land and the establishment of a pedestrian environment;
- c) Promote the efficient use of existing and planned *infrastructure* by creating the opportunity for various forms of commercial and *residential intensification*, where appropriate;
- d) Encourage the protection of existing commercial areas to ensure that their function as neighbourhood and community gathering places is retained;
- e) Encourage the continued revitalization of traditional and emerging main street areas which reflects their heritage significance and which promotes a mix of uses and attractions for retail, other community uses and activities, and tourism; and,

C1.4 PERMITTED USES IN URBAN AREAS AND RURAL SETTLEMENT AREAS

A full range of uses is permitted within *urban areas* in accordance with local Official Plan policies and land use designations. Within *rural settlement areas*, uses that are appropriate for *rural settlement areas* based on the nature of their servicing is also permitted, subject to the policies of the local Official Plans.

C1.5 HOUSING POLICIES

C1.5.1 Goals

It is the goal of this Plan to meet the County's current and future housing needs by:

- a) Monitoring and ensuring that there is a minimum 10 year supply of land for residential *development* in *urban areas* with sufficient water and wastewater capacity;
- b) Ensuring the provision of an appropriate range of housing types and densities to meet the needs of current and future residents;
- c) Assisting in the achievement of *residential intensification* and *affordable* housing by encouraging opportunities for mixed-use *development* in appropriate locations;
- d) Permitting the *development* of secondary suites;
- e) Encouraging the use of surplus public lands for *affordable* housing only if the site is appropriate for such a use and located where the use would be *compatible* with adjacent uses;
- f) Encouraging the *development* of seniors housing in the County;
- g) Encouraging the provision of alternative forms of housing for *special needs* groups;
- h) Supporting universal physical access and encourage the building industry to incorporate such features in new residential structures;
- i) Encouraging the *development* and *redevelopment* of lands within *settlement areas* and in appropriate locations at higher densities to maximize the use of *infrastructure*; and,
- j) Encouraging the *redevelopment* of brownfield properties and incentives to achieve it.

D1.12 WATERCOURSES

D1.12.1 Function of Watercourses

Watercourses that flow within the boundaries of the County are an integral part, and contribute to the health and function of the County's *natural heritage system* since they:

- a) Contain fish and *wildlife habitat* areas;
- b) Function as corridors for wildlife habitat movement and vegetation dispersal;
- c) Serve to maintain the quality and quantity of surface and groundwater resources;
- d) Assist in the improvement of air quality;
- e) Provide base flow and food supply to downstream areas; and,
- f) Provide stormwater conveyance and control.

D1.12.2 PROTECTION OF WATERCOURSES

- a) It is the intent of this Plan to recognize the importance of the ecological function of all watercourses, and their associated floodplains, valleys and stream corridors, which can serve as key components and linkages in the County's natural heritage system. Watercourses are generally shown on the schedules to this Plan.
- b) The County encourages the regeneration of natural areas near watercourses and the protection of headwater areas for maintaining natural hydrological processes within a watershed.
- c) New development in the form of buildings and structures and septic systems shall be located a minimum of 30 metres from the stable top of bank of a watercourse. This setback requirement must be met by all development unless more appropriate setbacks are recommended in accordance with an approved Subwatershed study, Environmental Impact Study or Geotechnical study in consultation with the appropriate Conservation Authority.
- d) In all cases where development is proposed adjacent to a watercourse, the approval authority shall be satisfied that the proposed development can be safely accommodated without there being a negative impact on the features and functions of the watercourse and its associated valley corridor.

recharge areas, identified by an appropriate agency, such as a local municipality, a conservation authority or the Ministry of Environment and Climate Change, shall be implemented by an amendment to this Plan.

- f) Nothing in this plan limits the local municipality in being more restrictive in the protection of municipal drinking water sources and aquifer/groundwater *vulnerable* areas.

D2.4 STORMWATER MANAGEMENT

- a) Planning for stormwater management shall:
 - i) Minimize, or, where possible, prevent increases in contaminant loads;
 - ii) Minimize changes in water balance and erosion;
 - iii) Not increase risks to human health and safety and property damage;
 - iv) Maximize the extent and function of vegetative and pervious surfaces; and,
 - v) Promote stormwater management best practices, including stormwater attenuation and re-use, and low impact development.
- b) In order to control flooding, ponding, erosion and sedimentation and to protect water quality and aquatic habitat or other natural habitat which depend on watercourses and other water bodies for their existence, stormwater management plans shall be required for any new development consisting of more than four lots or for commercial or industrial developments with large amounts of impervious area. Stormwater management will be undertaken in accordance with Ministry of Environment and Climate Change (MOECC) Guideline “Stormwater Management Planning and Design Manual, 2003.
- c) The County and the local municipalities shall require the use of stormwater management facilities downstream of new developments, where appropriate, to mitigate development impacts on stormwater quantity and quality.
- d) The development of naturalized stormwater management facilities, constructed with gentle slopes is promoted, and should be designed in accordance with the Ministry of the Environment and Climate Change guidelines.

E GENERAL DEVELOPMENT POLICIES

E1 SUBDIVISION OF LAND

E1.1 PURPOSE OF THIS SECTION OF OFFICIAL PLAN

This section contains policies that are to be considered with every application to develop land in the County through the subdivision, condominium and consent to sever processes. Regard shall also be given to the specific policies dealing with lot creation in each land use designation in addition to other policies in the Plan.

E1.2 CONDITIONS OF APPROVAL

The approval authority shall approve only those plans of subdivision, condominium or consent to sever applications, which conform to the provisions of this Plan and the applicable local Official Plan. Under conditions of approval attached to plans of subdivision or condominium pursuant to the Planning Act:

- a) The approval authority shall require that the applicant(s) enter into appropriate agreements with the County and/or local municipality which may be registered against the title of the subject lands and which shall include such matters as services, financial requirements, County road facilities, dedication of land for public uses, exclusive of parks and other requirements to implement the provisions of this Plan; and,
- b) The approval authority may require that the applicant(s) enter into appropriate agreements with local municipalities that shall be registered against the title of the subject lands, and may include such matters as, but not limited to, financial requirements, local roads, drainage, grading and landscaping, sidewalks and dedication of land for public uses and other requirements to implement the provisions of this Plan and the local Official Plan.

E1.3 WHEN A PLAN OF SUBDIVISION IS REQUIRED

Lot creation by Plan of Subdivision is generally required if:

- a) The extension of an existing public road or the *development* of a new public road is required to access the proposed lots; or,
- b) The extension of municipal water and/or sewer services is required to service the lands; or,
- c) The area that is proposed to be developed is not considered to be *infilling*; or,

- d) A Plan of Subdivision is required to ensure that the entire land holding or area is developed in an orderly and efficient manner; or,
- e) Unless otherwise specified in the local Official Plan, more than five lots including the retained lands are being created and/or the owner is retaining sufficient lands for the development of additional lots in accordance with the land use designation in the local Official Plan.

E1.4 SUBDIVISION REVIEW CONSIDERATIONS

Prior to the consideration of an application for Plan of Subdivision, the approval authority shall be satisfied that:

- a) The approval of the *development* is not premature and is in the public interest;
- b) There is *reserve sewage system capacity* and *reserve water system capacity* available in the municipal water and sewage treatment systems, if such services are to be provided;
- c) In areas without full municipal services, the plan can be serviced with an appropriate water supply and means of sewage disposal;
- d) The development will support the achievement of the minimum Greenfield density target for *urban areas* as set out in section B10 of this Plan;
- e) Stormwater management plans shall be required for any new development consisting of more than four lots or for commercial or industrial developments with large amounts of impervious area. Stormwater management will be undertaken in accordance with MOECC Guideline "Stormwater Management Planning and Design Manual, 2003.

The municipality shall require the use of stormwater management facilities downstream of new developments, where appropriate, to mitigate development impacts on stormwater quantity and quality. The municipality shall promote naturalized stormwater management facilities, constructed with gentle slopes. Applications for development shall be required to be supported by a stormwater quality/quantity management study.

- f) The lands will otherwise be appropriately serviced with *infrastructure*, schools, parkland and open space, community facilities and other amenities;
- g) The application, if approved, conforms to this Plan and the lower-tier Official Plan;
- h) The subdivision, when developed, will be appropriately integrated with other *development* in the area; and,

- i) The proposal has regard to Section 51 (24) of the Planning Act, as amended.

Prior to the registration of any Plan of Subdivision, a Subdivision Agreement between the landowner and the local municipality will be required.

E1.5 NEW LOTS BY CONSENT

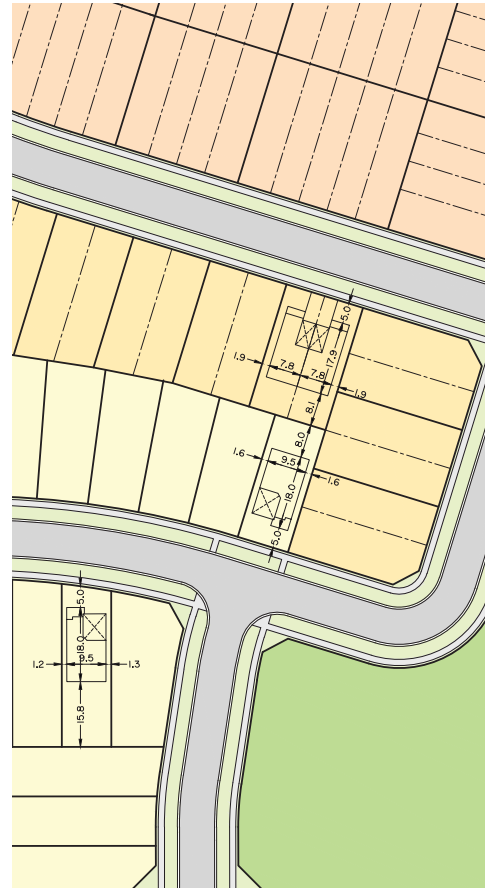
The approval of consents to sever land in the County of Northumberland shall be in conformity with the relevant policies contained in this Plan, policies contained in local Official Plans, and the provisions of the Planning Act.

E1.5.1 General Criteria

Provisional consent may be granted subject to appropriate conditions of approval for the severed and/or retained lot. Prior to issuing provisional consent for a new lot for any purpose, the approval authority shall be satisfied that the lot to be retained and the lot to be severed:

- a) Fronts on and will be directly accessed by a public road that is maintained on a year-round basis unless otherwise permitted in the local Plan;
- b) Does not have direct access to a County Road, unless the County permits a request for access;
- c) Will not cause a traffic hazard;
- d) Has adequate size and frontage for the proposed use in accordance with the local zoning by-law;
- e) Notwithstanding d) above, where a zoning by-law amendment or minor variance is required, approval of such amendment or variance shall be included as a condition of the approval of the consent;
- f) Can be serviced with an appropriate water supply and means of sewage disposal, provided there is confirmation of sufficient *reserve sewage system capacity* and *reserve water system capacity* within *municipal sewage services* and *municipal water services*;
- g) Will not have a *negative impact* on the drainage patterns in the area;
- h) Will not restrict the *development* of the retained lands or other parcels of land, particularly as it relates to the provision of access, if they are designated for *development* by this Plan;

*TOWN OF COBOURG OFFICIAL PLAN
EXTRACTS*



TOWN OF COBOURG

OFFICIAL PLAN

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within the Built Boundary is considered intensification and contributes to the intensification target in Section 3.2.3 of the Official Plan.

ii) Designated Greenfield Areas

The lands between the Built Boundary and the Town's Urban Settlement Area Boundary as designated on Schedule "A" are considered "designated greenfield areas". These are lands which are largely undeveloped. Development in these areas are required to contribute to the achievement of the minimum designated greenfield area density target in Section 3.2.3 of the Plan.

iii) Major Intensification Areas

Major intensification areas which shall be developed in accordance with all the applicable policies of this Plan include:

- Lands designated as "Mixed Use Areas" within the Built Boundary on Schedule "A" of the Official Plan with a particular focus on lands along Division Street south of the Major Transit Station;
- Lands designated "Employment Area" north of the Major Transit
- Unique Sites where significant existing land uses seek to redevelop
- Other vacant or underutilized sites, in particular larger, undeveloped

iv) Stable Residential Areas

Stable Residential Areas as designated on Schedule "A" consist of existing residential neighbourhoods where intensification potential would be modest and incremental.

v) Heritage Conservation Districts/Harbour Area

Significant areas of the Town, including the Main Central Area, form part of Heritage Conservation Districts and are subject to Heritage District Guidelines, while the Harbour Area is subject to detailed Secondary Plan policies. Intensification potential is extremely limited and subject to specific policies which reflect the special context of these important parts of the community.

3.2.3 Growth Management Strategy

In accordance with the Growth Management Strategy of the County of Northumberland and its Member Municipalities:

- i) The population allocation for the Town to the year 2031 is 4,220 additional people based on the Provincial allocation to the County, recognizing that the allocation is to be reviewed by the Province in 2011. The Town's population in 2006 was 18,210, as a result the allocated population in 2031 would be 22,430. The Town's current designated land exceeds the land required to accomodate this allocation.
- ii) The Town's housing mix target is 65% single detached housing and 35% multiples including semi-detached, townhouses and apartments.
- iii) The employment allocation for the Town to the year 2031 is 1,260 additional jobs based on the Provincial allocation to the County and 1 new job for every 3.94 new persons, recognizing that the allocation is to be reviewed by the Province in 2011. The related land area is 22.5 gross hectares. However, it should be noted that alternative projected employment for the Town based on 1 job per 2.4 new persons and alternative population projections identify the potential for between 2,000 and 4,000 new jobs to the year 2031. The related land area ranges from 36.94 to 71.77 gross hectares. The Town's current designated land exceeds the land required to accommodate these allocations, however, in accordance with the Growth Plan and recommendations of the County Growth Management Strategy, conversion of employment land to non-employment uses, including major retail uses, is discouraged and shall be subject to a comprehensive municipal review in accordance with the policies of Section 3.10.5.4 of this Plan.
- iv) In the Town's designated greenfield area as a whole, the density target is a minimum of 30 persons and jobs per hectare. This target is not intended, however, to be considered a minimum target on every parcel of land. In addition, increases in density for individual developments which conform to the other policies of this Plan shall be permitted.
- v) The Town's intensification target is for 39.34% of new residential dwelling units to be provided within the Built Boundary after 2015. Based on the Growth Management Strategy this development would generate a population of 1,665 people. However, intensification proposals which

<p style="text-align: center;">Table 1 <u>Land Uses Permitted In All Designations</u> (except in the Environmental Constraint Area designation)</p>	
Land Use	Related Conditions
<u>Wayside pits or quarries or portable asphalt plants for public road construction purposes</u>	<p>Wayside pits or quarries or portable asphalt plants for public road construction purposes shall be permitted on a temporary basis subject to the Aggregate Resources Act, the Ministry of Transportation Wayside Pits and Quarries Criteria and Ministry of the Environment guidelines. Further, the Town seeks active involvement with the Ministry of Natural Resources in the issuance of permits under the Aggregate Resources Act to ensure the following criteria are satisfied:</p> <ul style="list-style-type: none"> a) minimizes environmental disruption; b) incompatibilities with surrounding uses, particularly agricultural and residential uses, can be mitigated in an appropriate manner through the provision of buffering, engineering solutions or other similar approaches; and, c) appropriate controls are placed on the pit or quarry's location and rehabilitation.

3.4 RESIDENTIAL AREA

3.4.1 Purpose

The Residential Area designation on Schedule "A":

- i) recognizes established residential areas and ensures that new uses are generally compatible with the existing character and density of these areas; and,
- ii) provides for the creation of new residential areas which are generally compatible with the character and density of the existing residential areas.

3.4.2 Permitted Uses, Buildings and Structures

The permitted uses, buildings and structures are:

- i) low density residential including single detached, semi-detached and duplex dwellings; and,
- ii) medium density residential including townhouse dwellings, low rise apartments and stacked townhouses.

3.4.3 Land Use Policies

3.4.3.1 Stable Residential Areas

Stable residential areas are physically stable low density residential areas where potential new development or redevelopment is limited. Any intensification will be modest and incremental occurring through changes such as development of vacant lots and accessory apartments. Applications for new development in such areas shall be evaluated based on their ability to generally maintain the following elements of the structure and character of the immediate surrounding residential area:

- i) scale of development ~~with~~ respects ~~to~~ the height, massing and density of adjacent buildings and is appropriate for the site;
- ii) respects the nature of the streetscape as defined by such elements as landscaped areas, and the relationship between the public street, front yards and primary entrances to buildings;
- iii) respects the relationship between the rear wall of buildings and rear yard open spaces;
- iv) siting of buildings in relation to abutting properties ensures that there will be no significant negative impacts with respect to privacy and shadowing and appropriate buffering can be provided;
- v) conforms with density provisions of the Section 3.4.3.3;
- vi) conforms with the policies of Section 5.5, Cultural Heritage Preservation and preserves designated and listed cultural heritage buildings and structures, and where located adjacent to such buildings and structures is designed to be compatible;
- vii) respects the residential lotting pattern in the immediate surrounding area;

- ii) the existing lotting pattern;
- iii) boundaries created by physical features such as streams;
- iv) the prevailing building type including any special built form features; and,
- v) any special landscape or other features.

3.4.3.2 New Residential Areas

In new residential areas or significant redevelopment areas, applications for development shall be evaluated based on their conformity with the Growth Management Strategy in Section 3.2 and all other applicable policies of this Plan and the following criteria:

- i) a mix of development forms and densities;
- ii) medium density residential uses are encouraged and shall be :
 - a) intermixed with low density development in smaller groups;
 - b) primarily street oriented in design; and,
 - c) located adjacent to collector and arterial roads, park and greenland areas, community facilities and commercial areas and/or as a physical transition between high and low density residential development.
- iii) the road pattern is a modified, rectilinear grid pattern which provides for the maximum possible degree of connectivity internally, and externally with the existing developed areas and abutting arterial and collector roads with short blocks to promote active transportation modes; and,
- iv) the development incorporates linkages to the Town's greenland system and, incorporates private or public open space features or areas including Village Squares which serve as focal points for the residential development and/or structural elements which define the character and structure of the area.

3.4.3.3 Density

The density ranges for residential development shall be:

- i) Low Density
 - a) 12 units per net hectare (5 units per net acre) minimum
 - b) 20 units per net hectare (8 units per net acre) maximum
- ii) Medium Density
 - a) 20 units per net hectare (8 units per net acre) minimum
 - b) 50 units per net hectare (20 units per net acre) maximum.

3.4.3.4 Height

The maximum height for residential development shall be three storeys in Stable Residential Areas and four storeys in New Residential Areas.

3.4.4 Special Provisions

3.4.4.1 King Street West Village Area

The following special development criteria shall be considered by Council in reviewing any proposals for development or redevelopment in the King Street West Village area. This area is generally described as those lands situated between Burnham Street and Tracey Road, south of the railway corridor to Lake Ontario, excluding those lands west of Burnham Street to Maher Street.

In addition to the other development policies in this Plan, in the King Street West Village Area, the Land Use Plan shall be subject to the following:

- i) A continuous parkland area shall be provided adjacent to Lake Ontario. Acquisition of this parkland shall occur at the time of development of the lands through the subdivision, condominium, consent or site plan approval processes.
- ii) Architecturally significant, historically and/or contextually significant buildings within the area shall be retained wherever possible on

ii) Site Plan Review

Review of Site Plan Control applications for development projects on these lands will be subject to the guidelines of the Official Plan, particularly Section 4.5 Tree Conservation and Planting and Section 5, Community Design and Improvement, this section and other applicable Town of Cobourg planning, building and engineering criteria.

3.5 HIGH DENSITY RESIDENTIAL AREA

3.5.1 Purpose

The High Density Residential Area designation on Schedule “A”:

- i) recognizes established high density residential areas and ensures that new uses are generally compatible with the existing character and density of these areas and adjacent development; and,
- ii) provides for the creation of new high density residential areas in locations which are compatible with adjacent development.

3.5.2 Permitted Uses, Buildings and Structures

The permitted uses, buildings and structures are:

- i) medium density residential including townhouse dwellings, low rise apartments and stacked townhouses; and,
- ii) high density residential including high rise apartments.

3.5.3 Land Use Policies

3.5.3.1 Established High Density Residential Areas

Established high density residential areas are those areas where potential new development or redevelopment is limited. Applications for new development in such areas shall be evaluated based on their ability to

generally maintain the following elements of the structure and character of the surrounding high density residential area:

- a) the scale of development with respect to the height and massing of buildings;
- b) nature of the streetscape as defined by such elements as landscaped areas, and the relationship between the public street, front yards and primary entrances to buildings;
- c) relationship between the rear wall of buildings and rear yard open spaces;
- d) design and siting of buildings in relation to abutting properties, including any abutting lands in the Residential Area designation, to ensure that there will be no significant negative impacts with respect to privacy and shadowing and appropriate buffering can be provided;
- e) retention of the existing street pattern, unless modifications will improve accessibility for active transportation modes., and,
- f) any proposed redevelopment shall take into consideration adjacent uses including low density development, as well as adjacent development across a street.

A holding zone may be used to ensure that the appropriate review of new development is undertaken in accordance with these criteria.

3.5.3.2 New High Density Residential Areas

Applications for new High Density Residential Area designations shall be evaluated based on their conformity with the Growth Management Strategy in Section 3.2 and other applicable policies of this Plan and the following criteria:

- i) a mix of development forms and densities;
- ii) high density residential uses are:
 - a) intermixed with medium density development;

- b) primarily street oriented in design; and,
- c) located with direct access to collector and arterial roads, park and greenland areas, community facilities and/or commercial areas.
- iii) designed to ensure that there are no significant negative impacts with respect to privacy and shadowing, and that appropriate buffering can be provided for any adjacent lands in the Residential Area designation; and,
- iv) size and scale of the development is such that it can be integrated with any adjacent residential areas, in particular conforms with the policies of Section 5.5, Cultural Heritage Preservation and preserves designated and listed cultural heritage buildings and structures, and where located adjacent to such buildings and structures is designed to be compatible.

3.5.3.3 Density

The minimum density for residential development in the High Density Residential Area designation shall be 50 units per net hectare (20 units per net acre). The maximum density for residential development in the High Density Residential Area designation shall be 100 units per net hectare (40 units per net acre).

3.5.3.4 Height

The maximum height for residential development shall be six storeys. The minimum height shall be three storeys, other than a podium attached to a building may be two storeys.

3.5.4 Special Provisions

3.5.4.1 980 Burnham Street

Notwithstanding any other policies of this Plan, the maximum overall density for the property known as 980 Burnham Street shall not exceed 98 units per hectare (39.7 units per acre).

4.8 SUSTAINABILITY STRATEGY

4.8.1 Purpose

Consideration of the environment is not limited to the protection and enhancement of lands in the Greenlands System and the Town is committed to, and has already made significant progress in, developing a “culture of conservation” which reflects the principle of sustainable development - “development that meets the needs of the present without compromising the ability of future generations to meet their own needs.”¹

The Sustainability Strategy provides policies with respect to the implementation of this principle as it relates to development form as well as the framework for the creation of an Integrated Community Sustainability Plan for conservation of water, air quality, energy and other resources and waste reduction, transportation, as well as environmental monitoring and education. Related policies for cultural heritage conservation are found in Section 5.5 of the Plan.

4.8.2 Development Form

The Town will encourage, in accordance with the policies of this Plan and the Urban and Landscape Design Guidelines, development which is based on the principle of sustainable development, including Town facilities. The Town will also work with other public agencies to encourage them to follow the same principle. In particular, the Town will encourage development designed to:

- i) reduce the consumption of energy, land and other non-renewable resources including support for energy efficient building and opportunities for co-generation;
- ii) minimize the waste of materials, water and other limited resources;
- iii) create livable, healthy and productive environments; and,
- iv) reduce greenhouse gases.
- v) enhance biodiversity, ecological function, and the natural heritage system, including the provision of wildlife habitat and linkages.

¹The Bruntland Commission (1983)

5. COMMUNITY DESIGN ANDIMPROVEMENT

5.1 PURPOSE

Cobourg Official Plan is based on four community development principles including “Distinctive Community Image” and eight Design Principles. A key factor in the implementation of these principles is the design and improvement of the community.

This section outlines general design policies for the Town, as well as policies related to Gateway Areas and Neighbourhood Planning Areas, which will assist in the implementation of these broad directions. It also provides policies with respect to the preservation of heritage areas and community improvement.

The policies provide a framework for the review of development. In addition, the Town has adopted Urban and Landscape Design Guidelines. These shall be used, in conjunction with the policies of the Official Plan, as a basis for the evaluation of all development applications.

5.2 GENERAL DESIGN POLICIES

5.2.1 Streetscapes

5.2.1.1 Street System

- i) Street patterns in newly developing areas will be designed to reflect an interconnected street system with a modified grid pattern or other approaches which facilitate continuous and direct movement within a development area, and between the development area, abutting areas and the existing community. In addition, blocks should be limited in length to reduce the need for mid-block connections and promote active transportation.
- ii) Street patterns in existing areas shall not be modified except:
 - a) where the rights-of-way are significantly below Town standards and the need for widening has been identified in the 1992 Transportation Study or an update of that study,

recognizing that in Heritage Conservation Districts generally widenings shall not be permitted;

- b) to introduce traffic calming techniques;
- c) through the introduction of boulevards, wide sidewalks, landscaped medians or other techniques on arterial and collector roads to mitigate the visual impact of existing wide rights-of-way;
- d) the introduction of bicycle paths and lanes, as appropriate to the function of the road;
- e) the introduction of additional streets to improve connectivity within the street system; and,
- f) other modifications deemed necessary by the Town to improve the operation of the street.

5.2.1.2 Streetscape Design

The layout of streets, configuration of lots and the siting of buildings shall ensure that:

- i) there is no reverse lotting;
- ii) there is unobstructed street frontage adjacent to public open space and Environmental Constraint Areas, where appropriate, and subject to appropriate design to ensure that there will be no negative environmental impacts;
- iii) pavement, sidewalk and right-of-way widths, as well as the provision of bicycle lanes and paths, reflect the function of the street;
- iv) the siting of parking areas, and the siting and massing of buildings and structures on all streets, but particularly on arterial and collector streets, shall generally assist in the creation of high quality streetscapes and, in particular, regard shall be had to providing a sense of enclosure to the street by situating building mass adjacent to the street, and particularly at intersections, to frame the street space;

- v) in residential areas, garages shall be designed so that they are not the dominant feature in the streetscape. In particular, garages shall not:
 - a) project beyond the facade of the residence; and,
 - b) dominate the frontage of the lot,

unless plans are submitted by the applicant to demonstrate to the satisfaction of the Town that the garages can be appropriately integrated with the streetscape;
- vi) parking areas for non-residential uses or apartments or other large scale residential uses shall be designed to minimize areas where they directly front on the street, and where they do front on the street to reduce their visual impact both on the adjoining streetscape and on users by:
 - a) screening of the lot at the street through the use of such features as low fences, walls and a substantial landscaping buffer, excluding the area of any buildings and driveway crossings;
 - b) locating the building and parking on the site in a manner which reduces their impact on the street, and where buildings are located close to the streetline, no parking shall be permitted between the buildings and the street;
 - c) a reduction in the scale of large parking areas through their subdivision into smaller areas by means of landscaping; and,
 - d) joint access where feasible;
- vii) landscaping and/or architectural features provides for features such as the definition of the street and public open space, framing of views and focal points, direction of pedestrian movement and demarcation of areas with different functions as appropriate through features such as low masonry walls/ metal fences, rockery, special building and roofline treatments, landmark features (e.g. clocks, towers, cupolas, bays, pergolas, weather vanes, art), outdoor

activity areas (e.g. patios, plazas, squares), extensive plant material and other identifiable features; and;

- viii) service and open storage areas and utilities and services should be sited away from prominent views from the street to minimize impacts, alternative design options for locating such facilities should be considered, and only where there is no alternative should they be sited in view sensitive locations, in which case decorative screening shall be required.

5.2.2 Views

- i) New development shall be designed to preserve, enhance and/or create significant views of the following features:
 - a) Lake Ontario;
 - b) view from the Harbour to the Victoria Hall Clock Tower in accordance with the requirements of the Harbour Area Secondary Plan;
 - c) important institutional or other buildings;
 - d) open spaces; and,
 - e) natural features.
- ii) Public buildings and other major buildings and structures shall be encouraged to locate:
 - a) at the termination of a street or view corridor; and,
 - b) at the intersections of arterial or collector roads.

5.2.3 Areas of Heritage Value, Interest or Significance

- i) Designated Heritage Conservation Districts and properties shall be subject to the provisions of the Ontario Heritage Act and the Heritage District Guidelines where applicable. However, there are other areas in the Town which can be regarded as having heritage value, interest or significance, including areas identified in consultation with the Municipal Heritage Committee (Heritage

Cobourg), which the Town may establish as requiring special consideration with respect to the review of development applications. In those areas:

- a) the scale and massing of new construction shall be consistent with surrounding buildings to ensure a visual connection; and,
 - b) the general design and style of new construction shall be considered on an individual basis through the site plan approval process recognizing that contemporary styles may be more appropriate in certain cases than emulating turn of the century designs.
- ii) All new development permitted by this Plan, shall have regard for heritage resources and shall:
- a) incorporate these resources into any plan prepared for such new development; and,
 - b) avoid the removal or alteration of any historic material or distinctive architectural feature.

In the event that placement of a heritage resource precludes the reasonable development of a parcel, consideration may be given to accommodating the building elsewhere on the site or another location. Relocation of buildings shall only be permitted in accordance with the advice of the Municipal Heritage Committee (Heritage Cobourg) and approval of Council

- iii) In securing street extensions or necessary street improvements, consideration shall be given to the impact of such improvements on heritage resources and the urban streetscape.
- iv) The Town shall require the protection, conservation or mitigation of sites of archaeological value and areas of archaeological potential as provided for under the Planning Act, Ontario Heritage Act, and other Provincial legislation. Where development is proposed in areas of archaeological potential an archaeological assessment will be carried out in accordance with the requirements of Sections 5.5.5 and 8 of the Plan.

5.2.4 Landscape Design

The Town shall ensure that appropriate landscaping is provided in new development and redevelopment to:

- i) maintain and enhance the character of existing developed areas;
and,
- ii) allow for the creation of strong landscaped features in newly developing areas, including stormwater management facilities which shall be designed as important components of the Greenlands System, such that they will be incorporated in development in a manner which enhances views and access through street frontage;
and,
- iii) protect and enhance environmental features and open space areas having regard for the policies of Section 4, Greenlands System.

5.2.5 Safe Community Design

To promote safety and security in public places and minimize the potential for crime urban design should result in clear, unobstructed views of areas such as parking lots, parks, public institutions and open spaces, while streetscapes should be designed to encourage active public use and natural surveillance opportunities. In particular, the following measures shall be followed:

- i) the design and siting of new buildings and structures shall provide opportunities for visual overlook and ease of public access to adjacent streets, parks and open spaces;
- ii) clear, unobstructed views to parks and open spaces shall be provided from adjoining streets;
- iii) appropriate lighting, visibility and opportunities for informal surveillance shall be provided for all walkways, parking lots, parking garages and open space areas;
- iv) landscape elements shall be selected and sited in order to maintain views for safety and surveillance;
- v) the sharing of such facilities as parking and walkways shall be encouraged to increase use and public presence in such areas;

- vi) design which promotes a sense of community ownership for public spaces by maximizing use, control and surveillance opportunities by occupants of adjacent buildings and frequency of use by the public shall be encouraged;
- vii) the provision of views into, out of and through publicly accessible interior spaces shall be encouraged; and,
- viii) design which precludes entrapment or the perception of entrapment through properly identified exits and signage shall be encouraged.

5.2.6 Accessibility and Visitability

It shall be a policy of the Town to work towards the goal of equal access for all persons to public buildings and facilities, and to encourage the public and private sector to adopt similar goals with respect to their facilities as required to implement the Ontarians with Disabilities Act. The Town shall work with the Cobourg Accessibility Committee and citizens to continually refine and update a comprehensive, long term strategy for implementation which identifies barriers and proposes options to overcome them, allocates resources for education and change and monitors progress towards those ends. In particular, the principles of universal design should be applied to all public spaces and within new developments to ensure access and visitability for all individuals, while recognizing the need for balance where designated heritage properties or properties in Heritage Conservation Districts are involved. Regard shall also be given to the Ontario Building Code and the Ontarians with Disabilities Act, as well as the Town's Urban and Landscape Design Guidelines, the Heritage District Guidelines and, where appropriate, the Parks Canada Standards and Guidelines for the Conservation of Historic Places in Canada.

5.2.7 Active Transportation

In considering the design of public and private facilities, a key consideration shall be features which contribute to enhancements to the ability for movement by pedestrians and bicyclists and other active transportation modes, as well as individuals with mobility challenges while recognizing the need for balance in Heritage Conservation Districts. These include additions to the Town's trail system, the use of wide sidewalks, and the provision of bicycle parking and other facilities for

bicycles. In particular, in considering pedestrian movement on specific sites, the following shall apply:

- i) direct and clearly defined pedestrian connections which form an extension of the public transportation system shall be required throughout sites with one main pedestrian connection between all buildings and all adjacent municipal streets and sidewalks, and secondary connections for improved pedestrian access as appropriate;
- ii) principal pedestrian routes shall be functionally separated from parking and driveway areas and should utilize a variety of surface materials, textures, colour, landscaping and changes in elevation;
- iii) on large sites street furniture such as benches, waste receptacles, bicycle racks and pedestrian level lighting should be provided along main pedestrian routes where appropriate; and,
- iv) opportunities for pedestrian connections with adjacent sites should also be explored, and will be required wherever feasible.

5.2.8 External Building Design

When considering, as part of site plan approval, the external design of buildings and the design of the adjacent streetscape, the Town shall take into account all the policies of this Plan, particularly the policies of this Community Design Strategy. In addition, in evaluating the external design of buildings the Town shall seek design which reflects the directions in the Town's Urban Design and Landscape Guidelines and, in particular:

- i) includes sustainable building practices such as the use of green roofs and solar panels with the achievement of LEED or Energy Star or other similar certification or equivalent being encouraged;
- ii) complements the massing patterns, rhythms, character and context of adjacent existing development, while recognizing that built form evolves over time and that new buildings should not necessarily replicate existing buildings;
- iii) clearly defines public and private spaces;

- iv) is designed to incorporate the principles of universal design to ensure access and visitability for all individuals, while recognizing the need for balance where designated heritage properties and properties in Heritage Conservation Districts are involved;
- v) is in scale with surrounding development;
- vi) is designed to ensure that all buildings, and, in particular, any commercial, mixed use or institutional buildings are designed to animate and create a positive interface with the street through:
 - a) the use of architecture and facade treatment (e.g. landmark materials to reflect the heritage of the community;
 - b) front doors and generous real windows ,or in some cases three dimensional display windows, on any major walls facing streets;
 - c) strong pedestrian connections to the street, while drive-through uses, where permitted, are carefully positioned to minimize impacts on the street;
 - d) the location of outdoor activity areas (e.g. patios), landscaping and other site design elements.

5.2.9 Signage

Provisions for signs within private development shall comply with the Town's Sign By-law. In addition, the appearance of signs should reinforce the character of development through design and choice of colour, material and their placement at entrance areas and on building facades. Signs should be integrated into the site plan and building design to ensure coordination of design.

5.4 NEIGHBOURHOOD PLANNING AREAS

- i) The boundaries of the Neighbourhood Planning Areas are delineated on Schedule "C", attached to and forming part of this Plan. The Neighbourhood Planning Areas are primarily composed of low and medium density forms of housing, although in some cases there is provision for the development of high density residential dwelling types. The Neighbourhood Planning Areas delineated on Schedule "C" are largely developed.
- ii) It is intended that each Neighbourhood Planning Area will be developed with an identifiable focus which may include such facilities or services as a linked open space system, educational facilities, convenience commercial facilities or other similar services and facilities.
- iii) Prior to the development of undeveloped Neighbourhood Planning Areas, or prior, to the undertaking of any major redevelopment which would have the potential effect of substantially altering the pattern of land use within a Neighbourhood Planning Area, the Town may require the preparation of a Secondary Plan; and/or the following studies will be completed as a basis for the formulation of the appropriate form of development:
 - a) servicing;
 - b) stormwater management;
 - c) transportation;
 - d) environmental audit; and,
 - e) concept plan.

The studies or Secondary Plans shall be prepared in accordance with the policies of Section 8 of this Plan.

- iv) A Secondary Plan shall not be required within a predominantly developed Neighbourhood where development would be in the form of infilling and in conformity with the policies and intent of this Plan, and the designations set forth on Schedules "A" and "B".

5.5 CULTURAL HERITAGE CONSERVATION

5.5.1 Purpose

Act, but that Council believes to be of cultural heritage value or interest. Council shall consult with the MHC (Heritage Cobourg) prior to making modifications to the Register involving properties which are not designated. In addition to the provisions of the Ontario Heritage Act respecting demolition of cultural heritage resources contained in the Register shall apply.

5.5.6 Archaeological Resources

The Town recognizes that there may be archaeological remains of prehistoric and historic habitation, or areas containing archaeological potential in the municipality. The Town shall require the protection, conservation or mitigation of sites of archaeological value and areas of archaeological potential as provided for under the Planning Act, Ontario Heritage Act, and other Provincial legislation. Where development is proposed in areas of archaeological potential an archaeological assessment will be carried out in accordance with the requirements of Section 8 of the Plan. The Town recognizes that there may be a need for archaeological preservation in situ or rescue excavation of significant archaeological resources as a result of development proposals. Any alterations to known archaeological sites shall only be performed by licensed archaeologists.

5.5.7 Implementation

i) Grants and Loans

The Town may establish and maintain heritage grant and loan programs for the funding of the maintenance and conservation of cultural heritage properties for owners and /or long term lessees of designated heritage properties, properties in heritage conservation districts or properties with registered heritage easements. Such programs shall include guidelines to determine eligibility for funding and to advise on appropriate conservation techniques.

ii) Town Resources

The Town shall us protect and maintain all Town owned heritage resources to the highest standard to set a model for heritage conservation, while providing for adaptive reuse.

iii) Regulatory and other Legislative Tools

6. TRANSPORTATION STRATEGY

6.1 PURPOSE

The purpose of the Transportation Plan on Schedule “E”, and the policies of this Section, are to allow for the development of a transportation network which provides for the safe, economic and efficient movement of people and goods, while:

- i) offering a balance of transportation choices that reduces reliance on a single mode and promotes transit, cycling and walking and other active transportation modes;
- ii) recognizing the necessity for the creation of streetscapes which are attractive and comfortable for pedestrian as well as vehicular movement; and,
- iii) recognizing the importance of integrating transportation system planning and investment with land use planning, particularly the relationship of the land use pattern, density and mix of uses to the ability to minimize the length of vehicle trips and support public transit and active transportation modes.

To implement these directions the Town shall review and update its Transportation Master Plan with a focus on transit; pedestrian, bicycle and other active transportation modes; goods movement and improving connections to regional and inter-regional bus and rail services.

6.2 ROAD NETWORK

6.2.1 Road Classification

- i) The Provincial highway designation on Schedule “E” applies to Highway 401 which is under the jurisdiction of the Ministry of Transportation. The construction, maintenance and design of Highway 401 is not subject to the requirements of this Plan. However, the Town will review any proposed changes and provide input to the Province. In addition to the applicable municipal requirements, all proposed development located in the vicinity of Highway 401 within the Ministry of Transportation’s permit control area under the Public Transportation and Highway Improvement Act will be subject to their approval.

- ii) Major roads including ~~provincial highways~~, county roads, arterial and collector roads shall generally conform with the designations on Schedule “E” and the requirements of Table 3.
- iii) Potential major roads designated on Schedule “E” are established to reserve sufficient lands to provide choices for long term planning. Their alignments are approximate and may be modified without an amendment to this Plan. The construction of such roads shall comply with the requirements of Table 3.
- iv) Local roads while not designated on Schedule “E”, shall generally conform with the requirements of Table 3.
- v) Special roads shall be established by the Town, as set out in Table 3. and Section 6.2.2 iii). They may be designated on Schedule “E”, but are not required to be so designated. They shall be designed in accordance with a special study carried out by the Town.

6.2.2 Road Planning

- i) The Town shall work co-operatively with the Province and the County and adjacent municipalities to integrate the planning of the municipal road network with the road systems under other jurisdictions. Priority will be placed on ensuring that, in addition to vehicular movement and parking, the road system is designed to promote transit, cycling, walking and other active transportation modes, recognizing that Highway 401 is not designed for active transportation modes. The comfort and safety of transit users, pedestrians, cyclists and other active transportation mode users shall be an important consideration in streetscape design. In addition, priority will be given to transit and goods movement over other vehicular traffic.
- ii) The Town, as a condition of development or redevelopment, may require lands for the purposes of road widening to be dedicated to the appropriate authority having jurisdiction in accordance with the requirements of Table 3. Additional lands in excess of the typical right-of-way widths may also be required to be conveyed for works related to, but not limited to, extensive cut/fill operations, intersection improvements, bridges, sight triangles, and drainage and buffering improvements.

- iii) Notwithstanding the standards set out in this Plan, the Town recognizes that the reconstruction of roads to normal minimum standards in some existing developed areas may not be appropriate from a community design perspective, or economically or physically feasible due to existing building setbacks and/or mature trees within the road allowance. Any attempt to reconstruct such roads to minimize deficiencies shall only be undertaken after a study to determine a practical and reasonable right-of-way which will result in a streetscape which minimizes impacts on abutting properties and is appropriate to the character of the area, while serving anticipated traffic volumes. No amendment to the Plan shall be required to implement such a modification to the right-of-way.
- iv) Generally no new roads shall be assumed or dedicated which do not meet the minimum standards established in Table 3. However, the Town may accept roads with reduced standards, provided it is demonstrated to the satisfaction of the Town that such roads and lanes can serve the anticipated traffic volumes. In order to demonstrate the acceptability of such alternative road standards, the Town shall either carry out its own evaluation at the cost of the applicant, or require submission of an engineering study for their review. Such a review will be carried out at the cost of the applicant.
- v) New roads shall be designed to allow for the creation of tree-lined streets and to integrate “traffic calming” measures as appropriate, particularly in residential areas. In addition, where traffic problems are identified in existing areas through studies carried out by the municipality, the Town shall consider the introduction of “traffic calming” measures to assist in the resolution of such concerns.
- vi) The Town will work with the railways to maintain existing rights-of-way over the railway.

TABLE 3 FUNCTION OF ROAD FACILITIES		
ROAD CLASSIFICATION	FUNCTION	DESIGN REQUIREMENTS
County Roads and	• Serve inter-regional,	• Direct access from

Arterials	<p>regional and local travel demands, including movement of heavy trucks, transit and bicycles</p> <ul style="list-style-type: none"> Designed to serve development with densities supportive of transit and active transportation modes which are encouraged to locate along these roads. 	<p>abutting properties may be permitted subject to restrictions</p> <ul style="list-style-type: none"> Maximum 4 travel lanes <ul style="list-style-type: none"> Maximum right-of-way width 26 to 36 metres (85 - 118 feet), although generally will not exceed a maximum of 30 metres (98 feet)•
Collector Roads	<ul style="list-style-type: none"> Serve local travel demands providing connections within neighbourhoods and employment areas, Connects arterial and local roads and by doing so connects neighbourhoods and employment areas to other areas of the Town. 	<ul style="list-style-type: none"> Direct access from abutting properties permitted subject to some restrictions Maximum 4 travel lanes Maximum right-of-way width 20 -30 metres (66 -100 feet), although generally in residential areas the maximum right-of-way shall not exceed 26 metres. Right-of-way should be designed to include provision for wide sidewalks on both sides of the road(generally a minimum of 2 metres), street trees, and bike lanes.
Local Roads	<ul style="list-style-type: none"> Serves local and neighbourhood travel demands providing connections within neighbourhoods and employment areas to 	<ul style="list-style-type: none"> No access control Maximum 2 travel lanes Right-of-way width 20 metres (66 feet) maximum and minimum right-of-way width of 17 metres (55.7 feet)

	<p>ensure the maximum degree of connectivity to allow for the creation of an efficient network for all modes of transportation;</p> <ul style="list-style-type: none"> Connects individual properties to collectors and arterials 	<ul style="list-style-type: none"> Right-of-way should be designed to include provision for sidewalks at a minimum on one side of the street, with the exception of cul-de-sacs and other short streets, and street trees.
Laneways	<ul style="list-style-type: none"> Provides access to individual properties for parking and loading purposes may be public or privately owned 	<ul style="list-style-type: none"> No access control Right-of-way 7.5 metres (24.6 feet) width minimum
Special Streets	<ul style="list-style-type: none"> Serve a unique function related to their location, adjacent land use or other special feature which requires the development of special standards May be designated as determined by the Town without amendment to this Plan 	<ul style="list-style-type: none"> Design requirements will be determined based on a special study carried out by the Town

6.3 TRANSIT SERVICE

6.3.1 VIA Rail

The Town will work with VIA Rail to maintain and enhance existing rail passenger service to the community in accordance with the provisions of Section 6.5.

6.3.2

Municipal Transit Service

- i) The Town shall generally maintain its existing level of transit service and enhance and extend the service, if it is financially feasible to do so. No amendments shall be required to modify the transit system designated on Schedule “E”.
- ii) To enhance the potential for the success of the operation of the transit system, regard shall be had to the following transit-supportive guidelines in the subdivision review and approval process:
 - a) new subdivisions shall generally be located as close as possible to existing transit service;
 - b) contiguous development should be encouraged within new areas being built by multiple owners, so that planned transit service may be phased in efficiently along permanent routes;
 - c) development shall be encouraged at densities and in locations which support a cost effective transit service;
 - d) generally streets designated as transit routes should be designed to arterial or collector road standards and spaced to ensure full coverage and avoid duplication of service;
 - e) collector road linkages to enable connectivity between neighbouring residential areas shall be required where possible in new development; and,
 - f) where possible transit facilities should be located between 300 metres (984 feet) and 700 metres (2,296 feet) or less actual walking distance from residences.

6.4 PEDESTRIAN AND BICYCLE CIRCULATION

The Town, as set out in Section 6.1, shall promote transit, cycling and walking and other active transportation modes with the objective of establishing a transportation system throughout the community, that will allow residents to safely, conveniently and easily travel from anywhere to anywhere in Cobourg using active transportation modes.

- i) The primary system for pedestrian movement shall be the sidewalk system. Provision shall be made for sidewalks on both sides of all arterial and collector roads and on one side of all local streets, at a minimum, with the exception of cul-de-sacs, and streets with a limited number of homes on them or other special circumstances as determined by the Town.
- ii) Bicycle movement shall generally be accommodated in road right-of-ways. Bicycle lanes, in addition to those which form part of the link-node system, shall be included in road right-of-ways for new arterial and collector roads, and the addition of lanes for bicycles on existing arterial and collector roads shall be considered where it is financially feasible to do so. On major arterial roads such as King St. Division St., Elgin St., University Ave., William St., D'Arcy St., Ontario St., Brook Rd. North and Kerr St., the Town may also develop pathway systems for both pedestrian and bicycle traffic instead of bicycle lanes.
- iii) The Town shall encourage the development of a link-node system which will connect major pedestrian and bicycle destinations such as schools, parks and commercial areas, the Main Central Area, the harbour, and the train station with a system of pedestrian and bicycle paths including sidewalks and on-street bicycle lanes where appropriate.
- iv) Schedules "B" and "E" provide a conceptual framework for the development of the link-node system. Additions and modifications to the system shall not require an amendment to the Plan. The Secondary Plans will include appropriate extensions to the link-node system.

6.6 PARKING

6.6.1 General Parking Requirements

- i) The Town shall require, as a condition of development or redevelopment, that:
 - a) adequate off-street vehicular parking and loading facilities be provided which are planned and engineered to allow access to all parking spaces;
 - b) ingress and egress to parking and loading areas be limited in number and designed to acceptable standards for traffic safety; and,
 - c) access points on arterial and collector roads be shared by adjoining land uses, where practicable, to minimize traffic hazards.
- ii) Notwithstanding the Policies of Section 6.6.1 i), the Town shall encourage the retention and expansion of on-street parking in areas where it would not interfere with the efficient movement of traffic. Further, in areas where on-street parking is provided, the Town may consider reduction of the on-site parking standards.
- iii) Council may by by-law reduce the parking requirements in the Zoning By-law for a mixed use or high density residential development in accordance with a parking analysis as set out in Section 3.7.7 iv).
- iv) The Town shall establish in the Zoning By-law standards for bicycle parking and related facilities.

6.6.2 Main Central Area Parking Requirements

- i) The Town will maintain and enhance the supply of short term parking for shoppers and visitors within the Main Central Area and accessible parking for the short and medium term needs of residents and employees including parking for public uses.

7. MUNICIPAL INFRASTRUCTURE STRATEGY

7.1 PURPOSE

The Municipal Infrastructure Strategy establishes the type and level of infrastructure required to support the existing and proposed land use pattern, as well as the staging of development.

7.2 WATER SUPPLY AND DISTRIBUTION SYSTEM

7.2.1 Water Supply and Distribution System Capacity

- i) New development and/or redevelopment shall only be permitted where the water supply and distribution system has adequate capacity to service such development and/or redevelopment.
- ii) The expansion of the existing water system shall only be considered where:
 - a) strategies for water conservation and other water demand management initiatives are being implemented in the existing service area;
 - b) the plans are to serve growth in a manner that supports achievement of the Town's growth management strategy in Section 3.2;
 - c) the plans have been considered in the context of the applicable inter-provincial, national, bi-national, or state-provincial Great Lakes Basin agreements.

7.2.2 New Development

- i) All new development shall be serviced by the municipal water supply and distribution system and the developer shall be responsible for the installation of such works subject to the approval of the Town, Lakefront Utilities Services Inc. and the Ministry of the Environment.

The recommendations of any relevant watershed plan shall also be taken into consideration.

- ii) Notwithstanding the foregoing and the policies of Section 15.8, minor development in Cobourg East Community, which is not served by the municipal water supply and distribution system may be developed on a private systems, but not partial services, provided that the proposed development will not result in a potential health hazard, and is subject to the approval of the Town. Minor development in Cobourg East Community shall generally consist of development on existing lots of record or extensions of existing buildings and structures or similar changes. Minor development shall be development which requires only the following planning approvals:
 - a) a building permit; and/or
 - b) a variance; and/or
 - c) a site plan.

7.3 MUNICIPAL SEWAGE COLLECTION AND TREATMENT SYSTEM

7.3.1 Municipal Sewage Collection and Treatment System Capacity

- i) New development and/or redevelopment shall only be permitted where the sewage collection and treatment facilities are adequate to service such development.
- ii) In assessing the provision of sewage services, the Municipality shall have regard for the uncommitted hydraulic reserve capacity of both sewage plants and the Town's adopted Sewer Allocation Policy.
- iii) The expansion of the existing sanitary sewer system shall only be considered where the criteria in Section 7.2.1 ii) are satisfied.

7.3.2 New Development

- i) All new development shall be serviced by the municipal sewage collection and treatment system and the developer shall be responsible for the installation of such works subject to the approval

of the Town and the Ministry of the Environment. The recommendations of any relevant watershed plan shall also be taken into consideration.

- ii) Notwithstanding the foregoing and the policies of Section 15.8, minor development in Cobourg East Community which is not served by the municipal sewage collection and treatment facilities may be developed on private systems, but not partial services. Such development shall only be permitted provided that the proposed development will not result in a potential health hazard or further degradation of the environment and is subject to the approval of the Town. Minor development in Cobourg East Community shall generally consist of development on existing lots of record or extensions of existing buildings and structures or similar changes. Minor development shall be development which requires only the following planning approvals:
 - a) a building permit; and/or
 - b) a variance; and/or
 - c) a site plan.

7.4 STORM WATER MANAGEMENT

7.4.1 Existing Storm Water Facilities

- i) No development or redevelopment shall be permitted if such development or redevelopment will have a detrimental effect on the storm water drainage system.
- ii) The Town will co-operate with the Ganaraska Region Conservation Authority in watershed planning or in any flood management studies or engineering works that may be undertaken to improve, maintain or re-establish the hydraulic capacity of the natural water courses within the Town and on issues related to water quality. The Town shall use such studies to guide development decisions and water and wastewater servicing decisions.
- iii) No development will be permitted which would interfere with or reduce the drainage capacity of any natural watercourse, and, only

those works may be carried out in the watercourses which will improve their hydraulic efficiency, their attractiveness as open space areas in accordance with the policies of Section 4 of this Plan and result in no net loss of fisheries habitat.

- iv) The Town will ensure that all storm drainage facilities are constructed separately from sanitary sewer facilities and shall continue to provide for the separation of existing combined systems.

7.4.2 Midtown Creek Watershed

No works shall be undertaken which would result in a change in the size of the culverts beneath Elgin Street or the railway corridor or which would effectively reduce the storage capacity of the floodplain associated with the Midtown Creek unless new retention facilities are constructed south of Elgin Street or in other areas approved by the Town, in consultation with the Conservation Authority.

7.4.3 New Development

- i) Stormwater management techniques and facilities for new development shall be selected and designed on a subwatershed basis in accordance with Provincial guidelines, particularly the Ministry of the Environment "Stormwater Management Planning and Design Manual 2003 or updates thereto, and in consultation with the Ganaraska Region Conservation Authority. In particular stormwater management facilities shall be designed in a naturalized manner and will be landscaped so that they are integrated with the surrounding area and form part of the Greenlands System. The objectives of stormwater management with respect to new development within the Town of Cobourg shall be to:
 - a) identify appropriate water quality objectives for watercourse/drainage facilities to which the proposed developments are tributary with the primary focus being enhanced treatment;
 - b) identify and evaluate urban stormwater management practices to best meet these water quality objectives; and,

- c) select appropriate stormwater management practices for the site and provide preliminary design, siting and sizing of proposed facilities.
- ii) With respect to stormwater quantity control, development shall comply with the requirements of the approved Master Drainage Plan or stormwater plan. Where the Town requires the preparation of such plans they shall be prepared to the satisfaction of the Town at the expense of the applicant.
- iii) Where new stormwater management facilities (i.e. detention/retention ponds) are required to support development, they shall be located entirely within the boundaries of the Municipality.
- iv) Notwithstanding the foregoing policies, the Town may provide for, in consultation with the Conservation Authority, intensification, redevelopment and/or extensions to residential areas which are substantially developed without storm sewer facilities provided that the proposed development is integrated with the present system in a manner which will not adversely affect adjacent properties and individual lot level approaches to reduce runoff volume and treat stormwater on site or other innovative approaches to stormwater management are used to the extent possible.
- v) Individual lot level approaches are encouraged on all sites to reduce runoff volume and to treat stormwater on-site through Low Impact Development (LID) approaches including measures such as reduction of impervious areas, cisterns, porous or permeable pavement, green roofs, and bioswales.

7.4.4 Stormwater Management Studies

Stormwater management plans, master drainage plans, and other studies and works which involve the disposition of water, shall have regard for the maintenance of the natural ecological functions of the watercourses, waterbodies and surrounding lands which are subject to such works or studies.

8. DEVELOPMENT APPLICATION PRE-CONSULTATION AND SUBMISSION REQUIREMENTS

8.1 PURPOSE

All development applications in the Town of Cobourg shall be subject to review in accordance with the policies of this section, and the other applicable policies of this Plan, as well as the Town's Urban and Landscape Design Guidelines. In addition, the Town may require development to be subject to the site plan control provisions of the Planning Act.

8.2 PRE-CONSULTATION

8.2.1 Pre-Consultation Process

Consultation with the Town prior to the submission of a development application requiring Planning Act approval is encouraged, and shall be required for applications for approval of Official Plan amendments, Zoning By-law amendments, draft plan of subdivision, draft plan of condominiums and site plans. Affected agencies such as the Ganaraska Region Conservation Authority are encouraged to participate where appropriate.

8.2.2 Required Information and Materials

The Planning Act and its regulations prescribe the submission of certain information and materials as part of any development application. The Town may require information and materials to support any development application in addition to that prescribed, including maps, drawings, reports and technical studies. The specific requirements for an application to be deemed to be a "complete" application, shall be determined by the Town as part of the pre-consultation process, in consultation with the appropriate agencies.

8.3 REQUIRED INFORMATION AND MATERIALS

8.3.1 Information Requirements

Unless an exemption is granted by the Town in writing as part of the pre-consultation process, the following information and materials shall be required to be submitted as part of the application for an Official Plan amendment, zoning by-law amendment, draft plan of subdivision, draft plan of condominium and site plan, and may be required for other applications. Such required information and material shall be prepared at the applicant's expense and carried out by qualified professionals as determined by the Town.

i) Background Information

A plan or survey of the subject site identifying all existing significant trees and watercourses, and natural heritage features as listed in Section 4.2.1, which may include species at risk, significant habitat of endangered and threatened species, significant wetlands, significant woodlands, significant valley lands, significant wildlife habitat, and significant areas of natural and scientific interest, existing buildings and structures and contours.

ii) Plans

Plans, including, where applicable, site grading and landscaping plans, elevations, and other documentation such as 3D computer modelling, a shadow study and photo montages:

- a) describing the proposed development and its relationship to the existing streetscape in particular building envelopes, accesses and parking areas;
- b) demonstrating how stormwater, erosion and sedimentation will be controlled on site including the use of low impact development stormwater approaches; and,
- c) describing proposed landscaping and any modifications to existing significant trees and landscaping.

iii) Greenland System

Where the site includes or directly abuts lands designated on Schedule 'B', Greenland Area, proposals shall be evaluated in accordance with the information submission requirements of Section 4 of the Plan.

iv) Tree Analysis

Where new development is to occur on a piece of land, a landscaping and street furniture plan and an Arborist Report or similar report prepared by a qualified professional acceptable to the Town shall be submitted and approved by the Town either at the time of application or as a condition of approval at the direction of the Town in accordance with the policies of Section 4.5.2. Such a report shall include the identification of any tree or vegetation species at risk (e.g. Butternut Tree) and appropriate proposed mitigation (i.e. avoidance, buffers, setbacks).

v) Noise and Light Impact Studies

A noise impact study shall be carried out where residential uses or other sensitive uses, as defined by Ministry of the Environment guidelines, are proposed and the site is adjacent to a major noise source such as a Provincial Highway, arterial road, County Road, railway or industrial use. In particular, any proposed development within 500 metres (1,640 feet) of Highway 401 or a railway right-of-way may be required to undertake noise studies carried out by a qualified acoustical consultant to the satisfaction of the Town in accordance with Ministry of the Environment guidelines, and where applicable, in consultation with the appropriate rail company.

Further, the requirements of the Ministry of the Environment guidelines, "Noise Assessment Criteria in Land Use Planning, October, 1995" or any successor thereto, with respect to the need for noise impact studies shall apply to new commercial, industrial or institutional development which is a potential major noise source, such as, but not limited to, uses which have associated with them on-going construction activity, outdoor heat rejection systems (including cooling towers) and outdoor exhaust fans, or other stationary noise sources such as railways yards, major truck stops, major hydro

transformers, or natural gas compressors. Where required by the guidelines, a noise impact study shall be carried out to the satisfaction of the Town. In addition a noise impact study shall be required where drive-through uses abut residential or similar sensitive uses and a light impact study shall be required for commercial uses which abut residential or similar sensitive uses.

vi) Vibration Study

A vibration study shall be required at the request of the Province, the County, the Town or the applicable railway where the site is within 75 metres (246 feet) of a railway right-of-way or any Provincial Highway, arterial road or county road. Such a study shall be carried out to the satisfaction of the Town, in consultation with the appropriate government agency or railway.

Further, where new industrial development, which is a potential major source of vibration, such as metal forming industries including punch presses or drop forges, is proposed within 75 metres (246 feet) of existing residential development, a vibration study shall be carried out to the satisfaction of the Town.

vii) Cultural Heritage Impact Assessment

Where the site includes a building of architectural and/or historical merit designated under the Heritage Act or located in a designated Heritage Conservation District, a cultural heritage landscape or other cultural heritage resource, or the site is adjacent to such a cultural heritage resource as determined by the Town, a cultural heritage impact assessment, prepared by a qualified professional(s) in the field, shall be required to be submitted to the Town. Such a study shall demonstrate to the satisfaction of the Town that:

- a) the proposal will not adversely impact the heritage significance of the property or the area in which it is located or to which it is adjacent; or,
- b) demonstrate that it is not physically feasible to maintain the cultural heritage building or structure, landscape or resource, where that building, structure, landscape or resource is not designated under the Ontario Heritage Act.

viii) Archaeological Resource Assessment

Areas of archaeological potential are determined through the use of provincial screening criteria, or criteria developed based on the known archaeological record within the Town and developed by a licensed archaeologist. Where there may be archaeological remains of prehistoric and historic habitation, or areas containing archaeological potential within a site, an Stage I Archaeological Assessment conducted by archaeologists licensed under the Ontario Heritage Act shall be required. The assessment shall be submitted to the Town for approval, and to the Province for review and compliance with licensing provisions and archaeological assessment standards and guidelines. The assessment, should address further evaluation necessary to develop a final resource management strategy and the submission of any further reports required by the Town or the Province. In addition, the provisions of the Cemeteries Act and its regulations shall be applied when marked and unmarked cemeteries or burial places are encountered during development, assessment or any activity.

ix) Land Use Compatibility

Where applicable, information or necessary studies shall be provided to assist in the establishment of a separation distance, having regard for the Ministry of the Environment guidelines “Compatibility Between Industrial Facilities and Sensitive Uses”, between proposed development and potentially incompatible uses.

x) Stormwater Management and other Engineering Considerations

Where applicable, in accordance with Ministry of the Environment guidelines, a Master Drainage Plan shall be prepared, or the Town shall require a stormwater study. In addition, any other additional studies required to address engineering considerations may be required including:

- a) functional servicing study or servicing plan;
- b) hydrogeology, soils and/or geotechnical/slope stability study;
- c) hydraulic analysis of floodplain delineation;
- d) erosion and sediment control plan;
- e) water balance study, hydrological assessment, and/or water analysis report; and,

f) operation maintenance report

xi) Soil Quality

The applicant must demonstrate that on-site soil quality is suitable for the proposed use. Where site remediation is required the Town shall be satisfied with respect to the implementation of remediation prior to development approval. Specifically, where the Town, other public body, or the applicant has identified potential site contamination the Town shall require the applicant to complete a Phase I assessment, in accordance with the Ministry of the Environment's Guidelines for Use at Contaminated Sites in Ontario. The Phase I assessment will be reviewed by the Town to ensure there is no contamination prior to the granting of any development approvals. If there is evidence of contamination, the applicant will be required to submit a Phase 2 assessment in accordance with the Ministry of the Environment's Guidelines. If the site is found to be contaminated, the Town shall require the applicant to submit a Ministry of the Environment acknowledged Record of Site Condition to the Town to verify site clean-up prior to the granting of a building permit.

xii) Transportation Analysis

Where the Town has identified concerns with traffic impacts, a transportation analysis carried out by a qualified transportation engineer shall be required. This may deal with on- and/or off- site traffic impacts. In addition, for all proposed development, the proponent shall submit studies or other information which demonstrates how the development has been designed to accommodate pedestrians, bicyclists and other active transportation modes, as well as accessibility to transit.

xiii) Air Quality

Where the Town identifies concerns with impacts on air quality, including odours, appropriate studies by qualified professional shall be required.

xiv) Development Adjacent to Sewage Treatment Plants

Where development of any sensitive uses, including residential units, is proposed within 150 metres (492 feet) of a sewage treatment plant

with a design capacity equal to or less than 25,000 cubic metres per day, a feasibility study must be submitted in accordance with Ministry of the Environment's Guideline D-2 "Compatibility Between Sewage Treatment Facilities and Sensitive Land Uses." The feasibility study should address application of noise reduction equipment to any potential noise source, the degree and type of odour mitigation applied to the facility and other contaminants of concern.

xv) Development Adjacent to Highway 401

Where proposed development is located in the vicinity of Highway 401 within the Ministry of Transportation's permit control area under the Public Transportation and Highway Improvement Act, information will be provided:

- a) to satisfy the Town that the Ministry's requirements, that where feasible lots back onto the highway, can be satisfied; and,
- b) the Master Drainage Plan or stormwater study addresses impacts on the highway and has been submitted to the Ministry for approval.

xvi) Urban and Landscape and Sustainable Design

The submission of an urban and sustainable design study which identifies how the proposed development satisfies the Town's Community Design policies in Section 5 of this Plan, Official Plan policies related to sustainability and the Town's Urban and Landscape Design Guidelines shall be submitted with any development required by the Town. In addition, a shadow study and 3D computer modeling and other contextual documentation including photo montages, shall be required in support of the urban and sustainable design study.

xvii) Other Environmental Studies

All necessary studies shall be required to address environmental considerations including the environmental audit required by Section 4.2.6 and studies which address:

- a) demarcation of physical and stable top of bank;
- b) demarcation of the limits of natural heritage and hydrologic features;

- c) fish habitat assessment; and,
- d) natural features restoration plan.

Such studies may include an environmental impact study with terms of reference established by the Town in consultation with the Conservation Authority and/or the Ministry of Natural Resources.

xviii) Financial and Market Considerations

Financial and market considerations shall be addressed through the submission of the following studies:

- a) Market impact study;
- b) Financial impact study; and,
- c) Capital impact assessment.

xix) Density Reduction Justification Report

Applications that propose densities that are less than the minimums established in the Plan, and/or which require the down-zoning of sites which are zoned to permit medium and high density housing or mixed use development, which includes residential uses, to permit lower density housing are contrary to the policies of the Plan and while the Town may have to consider such applications they shall require an amendment to the Plan. Such applications shall be required to provide a Density Reduction Justification Report in accordance with Terms of Reference approved by the Town that assesses the implications of the proposal on the Town's ability to meet the intensification and density targets in this Plan and the impact on other sites zoned for medium and high density housing in the surrounding area. Such a study shall be carried out by a consultant retained by the Town at the cost of the applicant.

xx) High Voltage Electrical Corridors

Where new high voltage electrical corridors which are not located in a road allowance are proposed, studies shall be required which assess the impacts on adjacent development.

xxi) Other Information

In addition to the requirements of Sections i) -xviii), the applicant shall be required to submit any other supporting information and materials that may be identified by the Town during the pre-consultation process as being relevant and necessary to the evaluation of the particular application.

8.3.2

Scale, Scope and Timing of Submissions

- i) The scale, scope and timing of any required information and material, particularly any reports and technical studies, is dependent on the nature of the proposal, its relationship to the adjacent land uses and the type of planning approval required. It is also dependent on the natural features that exist on and adjacent to the subject site. An environmental audit, environmental impact study or other environmental studies, where required, must be completed at the appropriate time of year.
- ii) Where this Plan requires the submission of technical studies by the applicant in support of a development application, the Town may, at its discretion and after consultation with the applicant, require that such studies be carried out by a consultant retained by the Town at the cost of the applicant. The Town will generally not carry out peer reviews but may do so at its sole discretion. In either case, the applicant shall have input in the establishment of the terms of reference for such a study and a specific cost limit shall be established prior to the commencement of the study.

8.3.3

Complete Application

An application for an Official Plan amendment, Zoning By-law amendment, draft plan of subdivision or draft plan of condominium or site plan shall be considered complete under the Planning Act only when all the following items have been provided to the Town:

- i) an application form;
- ii) any information or materials prescribed by statute;
- iii) a pre-consultation form;
- iv) any supporting information or materials required to be provided in accordance with Section 8.3.1; and,

- v) the prescribed application fee.

8.3.4 Development Evaluation Criteria

Development proposals shall generally conform with the following criteria, in addition to any of the applicable policies of this Plan:

- i) Design and Sustainability

The appropriateness of the design and sustainability of the proposal shall be evaluated in accordance with the policies of Section 5 and the Town's Urban and Landscape Design Guidelines. In particular, where new development is proposed abutting existing residential development it shall be designed to be compatible with that existing development with respect to scale and massing. Adequate buffering will also be provided. Consideration will be given to shadow studies and 3D modelling in assessing the compatibility of such proposals. In addition, development may be subject to the site plan control provisions of the Planning Act.

- ii) Heritage Preservation and Archaeological Protection

Buildings, landscapes and other resources containing cultural heritage value or interest are preserved on site and/or integrated into the development in an appropriate manner and/or preserved in some other manner. Standard archaeological potential criteria in accordance with Ministry of Tourism and Culture guidelines will be used to evaluate future development areas and expansion proposals for impacts on significant resources. Recommended processes and procedures for archaeological management will be implemented through a variety of measures including, but not limited to, the mitigation, preservation, and/or resource excavation, removal and documentation, of all archaeological resources to the satisfaction of the Town and approval of the Province, and taking into consideration dialogue with the First Nations and their interests.

- iii) Greenland System

Development shall only be permitted in areas designated on Schedule 'B' as part of the Greenland System in accordance with the

policies of Section 4 of this Plan in particular regard shall be had to the provisions of Section 4.2.

iv) Watercourses

All watercourses shall be, where feasible, maintained or enhanced as distinct ecosystems, and lands immediately adjacent to these watercourses shall be retained or rehabilitated to a natural self-sustaining state. Alterations to watercourses, including riparian features such as intermittent streams and drainage swales, shall generally be discouraged. However, the necessity for retention /restoration of riparian features such as intermittent streams and drainage swales, will be evaluated on a site by site basis and some modifications of these features may be approved where deemed appropriate by the Town, in consultation with the Conservation Authority.

v) Sewer and Water Services and Stormwater Management

Development shall be serviced in accordance with the provisions of Section 7 of this Plan.

vi) Soil Quality

The applicant must demonstrate that on-site soil quality is suitable for the proposed use in accordance with the provisions of Section 8.3.1 xi). Where site remediation is required the Town shall be satisfied with respect to the implementation of remediation prior to development approval.

vii) Noise and Vibration Impact and Safety Measures

Where the site is adjacent to a major noise source such as a Provincial Highway, arterial road, County Road, railway or industrial use, the Town, in consultation with the appropriate government agency and, if applicable, the appropriate railway, shall be satisfied that appropriate measures to mitigate any adverse effects of noise and vibration can be implemented. Furthermore, for sites adjacent to a railway, the Town, in consultation with the applicable railway, shall be satisfied that appropriate safety measures such as setbacks, berms and security fencing are provided.

viii) Transportation

The applicant must demonstrate that the existing road system can accommodate any traffic impacts or can be modified to do so at the cost of the applicant. In addition, the applicant must demonstrate how the development has been designed to promote access and use by pedestrians, bicyclists and other active transportation modes, as well as accessibility to transit.

ix) Air Quality

Where the Town identifies potential concerns with air quality, the applicant must demonstrate that any impacts on air quality can be appropriately mitigated at the cost of the applicant.

x) Development Adjacent to Sewage Treatment Plants

The applicant must demonstrate that any impacts arising from proximity to a sewage treatment plant can be appropriately mitigated in accordance with Ministry of the Environment Guidelines.

xv) Development Adjacent to Highway 401

The applicant must demonstrate that the requirements of the Ministry of Transportation in their permit control area related to Highway 401 have been satisfied.

xvi) Density Reductions

Applications that propose the down-zoning of sites which are zoned to permit medium and high density housing or mixed use development which includes residential uses to permit lower density housing shall be generally be discouraged. The Town may consider approval of such an application if the required Density Reduction Justification Report demonstrates that the reduction in density will not significantly affect the Town's ability to achieve its intensification and density targets and/or that the downzoning is compensated for by the creation of additional medium or high density residential development elsewhere in the Town

If Council has not passed a by-law under the Planning Act subsequent to the completion or review of the study within the period of time specified in the Interim Control By-law, the provisions of any zoning by-law passed under the Act that applied to the subject lands immediately prior to the coming into force of the Interim Control By-law again come into force.

9.3 FURTHER AMENDMENTS TO PLAN

In considering an amendment to the Plan, regard shall be had to the following criteria which are in addition to those specified in the remainder of this Plan:

- i) The need for the proposed use;
- ii) The extent to which the existing areas in the proposed designations are developed, and the nature and adequacy of such existing development;
- iii) The physical suitability of the land for such proposed use;
- iv) The location of the areas under consideration with respect to:
 - a) the adequacy of the existing and proposed road system in relation to the development of such proposed areas;
 - b) the convenience and accessibility of the site for vehicular and pedestrian traffic and traffic safety in relation thereto; and
 - c) the adequacy and availability of the municipal water and sewage facilities, and other municipal services in view of the policies contained in this Plan and in accordance with technical reports or recommendations submitted by the applicant or agencies.
- v) The compatibility of such proposed use with uses in the surrounding area; and,
- vi) The potential effect of the proposed use on the financial position of the Town.

provincial interests;

- ii) utilizes opportunities to accommodate projected growth through intensification and redevelopment;
- iii) is integrated with planning for infrastructure and public service facilities; and,
- iv) considers cross-jurisdictional issues.

Density, Gross

This term shall mean the area of land including the lot area, local and collector streets, parks, including trails, schools, institutional uses and all open space lands with the exception of lands designated “Environmental Constraint Area” on Schedule “A”.

Density, Net

This term shall mean the area of land including the lot and local roads.

Environmental Buffer

A natural area intended to minimize potential conflict between human activities and sensitive environmental features. This is done by providing a natural separator and by rounding out irregularities and minimizing the length of the interface between humans and undisturbed, regenerating or protected landscapes.

Floor Space Index

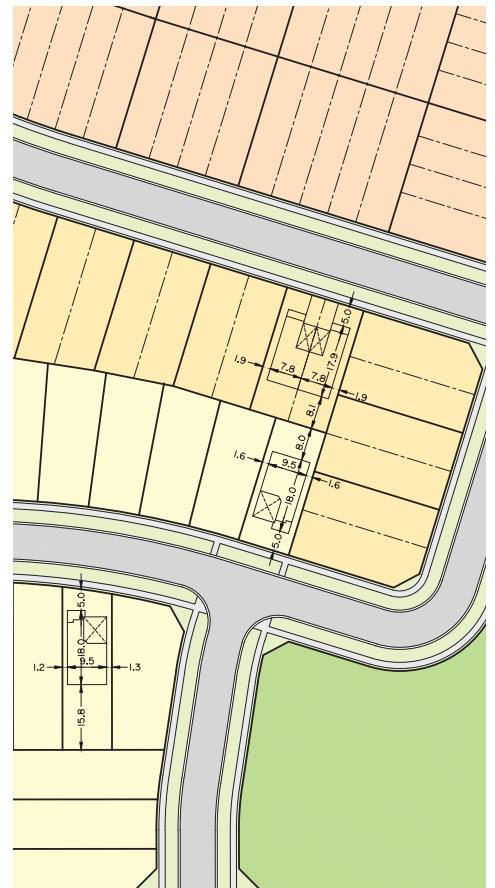
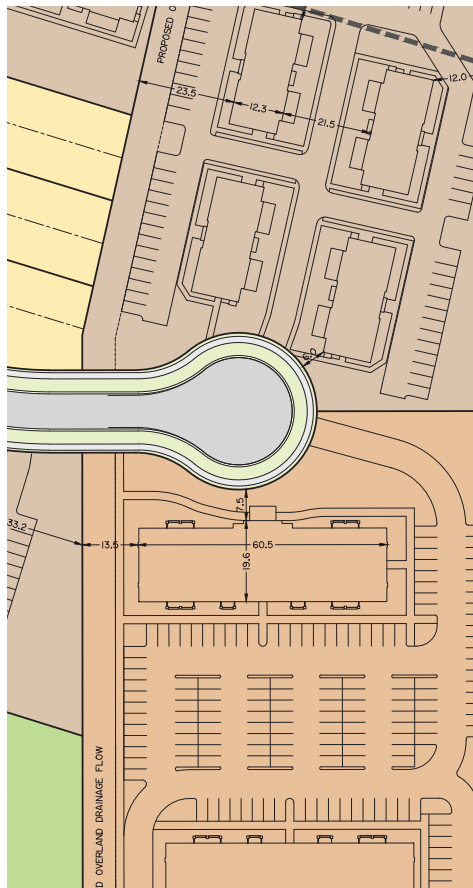
The gross area of all buildings on a lot divided by the lot area.

Garden Suite

A garden suite is a small independent building, physically separate from the principal dwelling unit with which it is associated, which may be used as a dwelling unit, or for activities accessory to those permitted in the principal dwelling unit, and which may have a primary access from a rear lane abutting the lot upon which both the garden suite and its associated principal dwelling unit are located.

APPENDIX 'E'

ZONING BY-LAW EXTRACTS





THE CORPORATION OF THE TOWN OF COBOURG

BY-LAW NUMBER 022-2018

A BY-LAW TO AMEND ZONING BY-LAW NUMBER 85-2003 (Select Lands within East Village Phases 3 & 4)

WHEREAS the Municipal Council of the Corporation of the Town of Cobourg convened a Public Meeting in accordance with the Planning Act, R.S.O. 1990, c.P. 13, as amended, on May 14, 2018 regarding an application for a Zoning By-law Amendment for select lands within East Village Subdivision, Phases 3 & 4;

AND WHEREAS one public submission in favour of the application was received by the Council of the Corporation of the Town of Cobourg at the Public Meeting;

AND WHEREAS the Council of the Corporation of the Town of Cobourg duly considered the public submission and all other relevant information surrounding the subject matter, and now deems it advisable to amend By-Law Number 85-2003, as amended;

NOW THEREFORE the Municipal Council of the Corporation of the Town of Cobourg enacts as follows:

1. That By-law No. 85-2003, Section 10.2. is hereby amended by the addition of the following subsections:

"10.2.27 **Multiple Residential 4 Exception 29 (R4-29) Zone – Lands within East Village Subdivision, Phases 3 & 4**

10.2.27.1 **Defined Area**

R4-29 as shown on Schedule 'A', portion of Map 5 to this By-law

10.2.27.2 **Permitted Uses and Permitted Accessory Uses**

The uses permitted shall be in accordance with Section 10.1.1 and 10.1.2.

10.2.27.3 **Permitted Buildings and Structures**

- i) one single detached dwelling on one lot;
- ii) one unit of a semi-detached dwelling on one lot;
- iii) one semi-detached dwelling on one lot;
- iv) townhouse dwelling;
- v) accessory buildings and structures for the permitted uses;
- vi) buildings and structures for public uses in accordance with the provisions of Section 5.3.2

15.2.27.4 **Regulations for Permitted Uses in the R4-29 Zone**

The regulations of Section 10.1 shall apply to the permitted uses in the R4- 29 Zone, with the exception of the following:

Lot Area

- | | | |
|------|---------------------------------------|--|
| i) | single detached dwelling: | 370 sq m (3,982 sq ft) minimum with the exception that up to 2 lots may be 337 sq m (3,627 sq ft) minimum and up to 21 lots may be 363 sq m (3907 sq ft) minimum |
| ii) | one unit of a semi-detached dwelling: | 278 sq m (2,992 sq ft) minimum |
| iii) | one semi-detached dwelling: | 556 sq m (5,985 sq ft) minimum |
| iv) | townhouse dwelling: | 215 sq m (2,314 sq ft) minimum, with the exception that up to 24 lots may be 192 sq m (2,066 sq ft) minimum and up to 8 lots may be 202 sq m (2,174 sq ft) minimum |

Lot Coverage

- | | | |
|------|---------------------------|-------------|
| i) | single detached dwelling: | 45% maximum |
| ii) | semi-detached dwelling: | 50% maximum |
| iii) | townhouse dwelling: | 55% maximum |

Front Yard

2.5 m (8.2 ft) minimum up to 5.0 m (16.4 ft) maximum, subject to the following provisions:

- i) The wall of an attached garage facing the public street shall not be located closer to the front lot line than either the front porch or the main front entrance of the dwelling unit, whichever is closer to the street.
- ii) The front wall of the dwelling above an attached private garage shall be located no further than 2.5 metres from the front wall of the attached private garage.
- iii) The wall of an attached private garage facing the public street shall be setback a minimum of 5.8 metres from the lot line that the driveway crosses to access the private attached garage. If the driveway does not cross a sidewalk, the minimum setback for the wall of the attached garage shall be 4.5 metres minimum.

Garage Width

The maximum width of an attached garage shall be 4.0 m (13 ft), with the exception that, on a lot equal to or greater than 12.0 m frontage, an attached garage may be a maximum of 5.5 m in width.

Driveway Setback

The minimum setback from a side lot line to a driveway shall be 0.6 m (2.0 ft).

Interior Side Yard

- i) single detached dwelling: 0.75 m (2.5 ft) minimum for a one storey dwelling and 0.45 m (1.5 ft) for each additional storey.
- ii) semi-detached dwelling and townhouse dwelling: 1, 2 m
no interior side yard shall be required between the common vertical wall dividing one unit from another. The side yard on the other side of the unit shall be in accordance with the provisions for a single detached dwelling.

Exterior Side Yard

3.0 m (9.8 ft) minimum

Landscaped Open Space

Minimum 30% of the lot

Building Height

- i) single detached dwelling and semi-detached dwelling: 2 storeys maximum
- ii) townhouse dwelling: 3 storeys maximum


Regulations for Accessory Buildings and Structures

In accordance with the provisions of Section 10.1.19, with the exception that the total lot coverage shall not exceed 55% of the lot area.

2. That Schedule 'A', Map 5, attached to and forming part of By-law No. 85-2003, is hereby amended by changing the zone category of the lands as illustrated on Schedule "B" attached hereto from "Multiple Residential 4 Exception Nineteen (R4-19) Zone" to "Multiple Residential 4 Exception Twenty-Nine (R4-29) Zone."

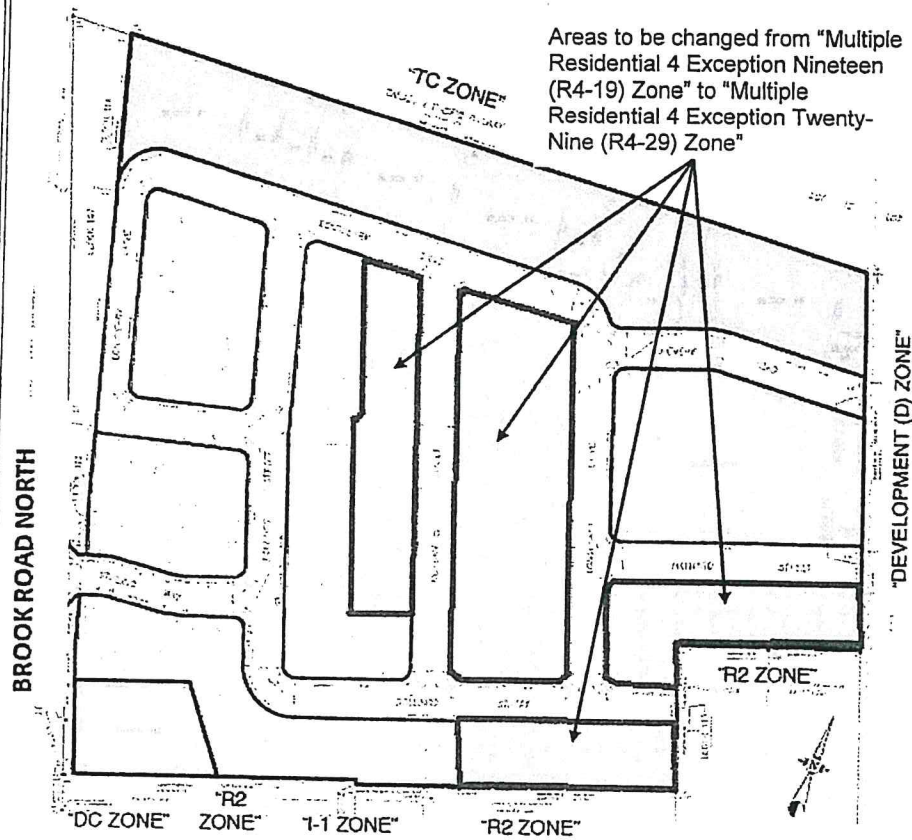
2. That Schedule "B" attached hereto is hereby made part of this by-law as fully and to all intents and purposes as though recited in full herein.
3. THIS BY-LAW shall come into force and effect upon final passing hereof, subject to the provisions of the Planning Act, R.S.O. 1990, c. P. 13.


READ and passed in Open Council this 22nd day of May, 2018.


MAYOR


MUNICIPAL CLERK

Schedule "B"



 <p>COBOURG</p>	<p>THE CORPORATION OF THE TOWN OF COBOURG</p>
	<p>BY-LAW NUMBER <u>068-2012</u></p>

A BY-LAW TO A BY-LAW TO AMEND ZONING BY-LAW NUMBER 085-2003 (Blocks 92 to 96 and Lots 85 to 91, PLAN 39M-875)

WHEREAS the Council of the Corporation of the Town of Cobourg deems it advisable to amend By-Law Number 85-2003 as amended;

NOW THEREFORE the Council of the Corporation of the Town of Cobourg enacts as follows:

1. That By-law No. 85-2003, Section 10 is hereby amended by the addition of the following new subsection:

10.2.26 Multiple Residential 4 Exception 26 Holding (R4-26[H]) Zone"

10.2.26.1 Defined Area:

(R4-26[H]) as shown on Schedule 'A', to this by-law.

10.2.26.2 Permitted Uses:

The uses permitted in Section 10.1.1 and 10.1.2

10.2.26.3 Permitted Buildings and Structures

The buildings and structures permitted in Section 10.1.3 in addition to the following:

- i) two multiple dwellings on one lot, including dwellings for senior citizens and/or the disabled

10.2.26.4 The regulations of Section 10.1 shall apply with the exception of the following:

i) Density – Multiple Dwelling

10 dwelling units maximum per building

ii) Lot Frontage

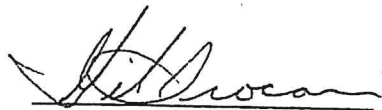
Two multiple dwellings on one lot: 75 m min.

iii) Front Yard

3 m (9.8 ft.) minimum

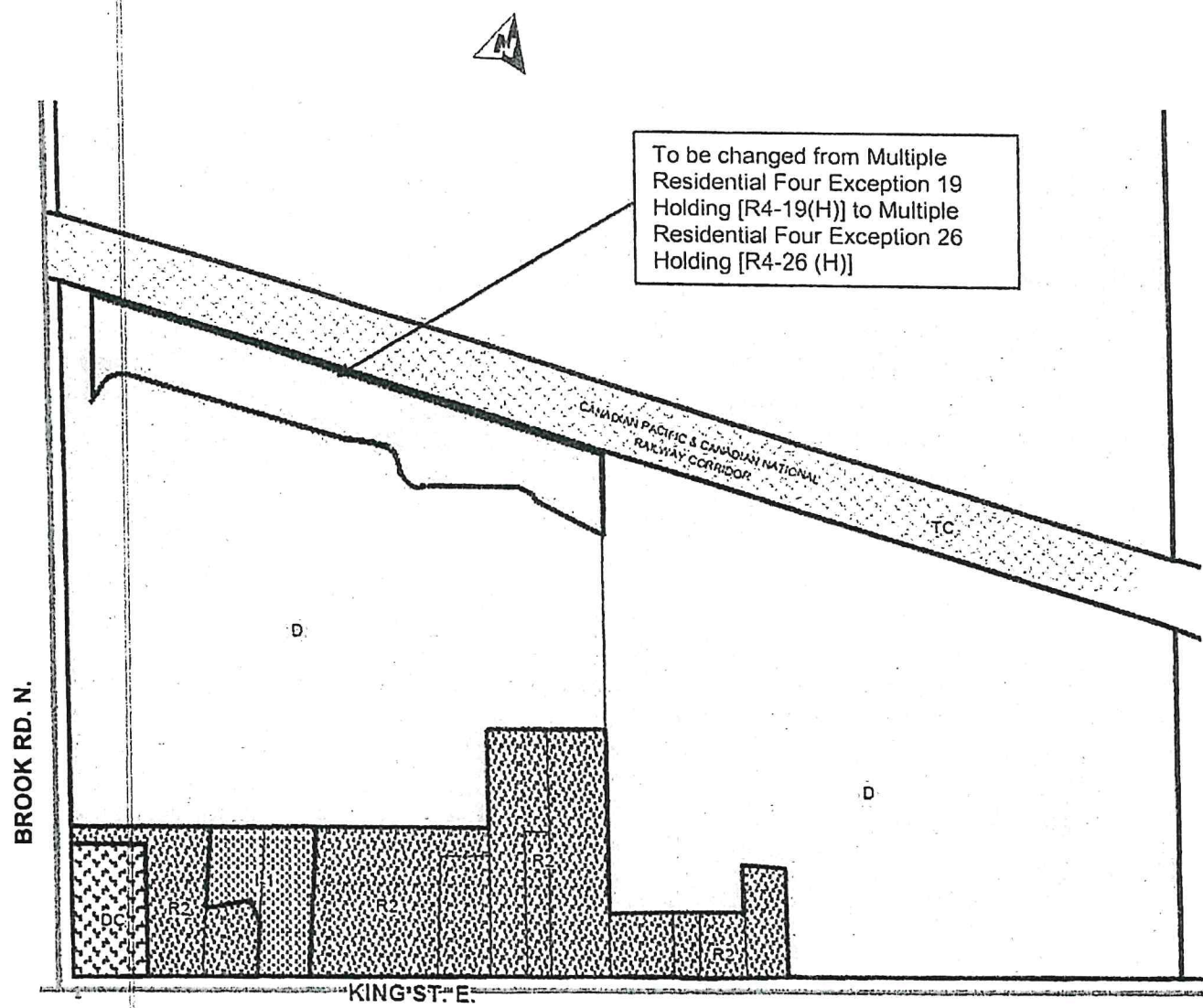
2. That Schedule 'A', Map 5, attached to and forming part of By-law No. 85-2003, is hereby amended by changing the zone category of the lands known municipally as Blocks 92 to 96 and Lots 85 to 91, PLAN 39M-875 from "Multiple Residential 4 Exception 19 Holding (R4-19[H]) Zone" to "Multiple Residential 4 Exception 26 Holding (R4-26[H]) Zone" as illustrated on Schedule "B" attached hereto.
3. That Schedule "B" attached hereto is hereby made part of this by-law as fully and to all intents and purposes as though recited in full herein.
4. THIS BY-LAW shall come into force and effect upon final passing hereof, subject to the provisions of the Planning Act, R.S.O. 1990, c. P. 13.

READ a first, second and third time and finally passed in Open Council this 17th day of September, 2012


MAYOR


MUNICIPAL CLERK

SCHEDULE "B"





TOWN OF COBOURG

OCTOBER 2003

ZONING BY-LAW NO # 85-2003

Passage: October 14, 2003

Revised: March 8, 2004

Important Note: This document is for reference purposes only and contains only those amendments approved prior to and including March 8, 2004. Always confirm the accuracy of any data retrieved from this document with the Planning Department.

SECTION 10: MULTIPLE RESIDENTIAL 4 (R4) ZONE REGULATIONS

10.1 GENERAL USE REGULATIONS

10.1.1 Permitted Uses

- i) public use in accordance with the provisions of Section 5.3.2;
- ii) residential use.

10.1.2 Permitted Accessory Uses

- i) accessory use to the Permitted Uses under Section 10.1.1;
- ii) home occupation use;
- iii) one bed and breakfast establishment.

10.1.3 Permitted Buildings and Structures

- i) one unit of a semi-detached dwelling on one lot;
- ii) one semi-detached dwelling on one lot;
- iii) one duplex dwelling on one lot;
- iv) one triplex dwelling on one lot;
- v) one converted dwelling on one lot;
- vi) one four-plex dwelling on one lot;
- vii) townhouse dwelling including a townhouse dwelling for senior citizen's and/or the disabled;
- viii) one multiple dwelling on one lot including a multiple dwelling for senior citizens and/or the disabled;
- ix) one rooming or boarding house containing no more than ten guest rooms on one lot;
- x) one apartment dwelling including apartments for senior citizens and/or the disabled;
- xi) buildings and structures for public uses in accordance with the provisions of Section 5.3.2;
- xii) accessory buildings and structures for the permitted uses.

10.1.4 Lot Area

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|---|---|
| i) one unit of a semi-detached dwelling; | 278 m ² (3000 ft. ²) minimum |
| ii) one semi-detached dwelling on one lot,
or one duplex building on one lot; | 557 m ² (6,000 ft. ²) minimum |
| iii) triplex building; four-plex building;
converted building; a rooming or
boarding house; or multiple dwelling; | 650 m ² (7,000 ft. ²) minimum |
| iv) townhouse dwelling; | 215 m ² (2314 ft. ²) minimum per dwelling unit |
| v) apartment dwelling; | not applicable |

10.1.5 Lot Frontage

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|---|-----------------------|
| i) one unit of a semi-detached dwelling | 9 m (30 ft.) minimum |
| ii) one semi-detached building on
one lot; or a duplex building, | 18 m (60 ft.) minimum |

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|------|--|---------------------------------|
| iii) | triplex building, converted building, a four-plex, or a multiple building on one lot | 18 m (60 ft.) minimum |
| iv) | townhouse dwelling which does not front onto a public street | 30 m (100 ft.) minimum |
| v) | townhouse dwelling where each townhouse unit fronts onto a public street | 6.5 m (21 ft.) per unit minimum |
| vi) | apartment dwelling | 30 m (100 ft.) minimum |

10.1.6 Lot Coverage

40% maximum.

10.1.7 Net Density - Apartment Dwelling/Converted Building/Multiple Building

50 units per net hectare (20 units per net acre) maximum.

10.1.8 Floor Area Per Dwelling Unit:

Type of Dwelling Unit	Minimum Floor Area
Bachelor Unit	28 m ² (300 ft. ²)
One Bedroom Unit	42 m ² (450 ft. ²)
Two Bedroom Unit	56 m ² (600 ft. ²)
Three Bedroom Unit	70 m ² (750 ft. ²)
Each Additional Bedroom	10 m ² (107 ft. ²)
Dwelling, Senior Citizen - Bachelor Unit	27 m ² (290 ft. ²)
Dwelling, Senior Citizen - One Bedroom Unit	40 m ² (430 ft. ²)
Dwelling, Senior Citizen - Two Bedroom Unit	48 m ² (516 ft. ²)
Dwelling, Senior Citizen - Three Bedroom Unit	62 m ² (667 ft. ²)

10.1.9 Front Yard

The established building line or where not applicable, 6 m (20 ft) minimum.

10.1.10 Rear Yard

7 m (23 ft.) minimum.

10.1.11 Interior Side Yard

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|----|--|---|
| i) | one unit of a semi-detached dwelling, or a townhouse unit; | no interior side yard shall be required between the common vertical wall dividing one dwelling unit from another. The side yard on the other side of the unit shall be 1.5 m (5 ft.) minimum and 1 m (3.3 ft) for each additional storey; |
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| ii) | a duplex building; one semi-detached building on one lot; one converted, two unit building: | 2.5 m (8 ft.) minimum for each interior side yard. |
| iii) | one triplex, an apartment building, a four-plex building, a multiple building, one converted building with more than two, residential units or a rooming and boarding house: | 3.65 m (12 ft.) minimum. |

10.1.12 Exterior Side Yard

The established building line, or where not applicable, 6 m (20 ft.) minimum.

10.1.13 Distance Between Exterior Walls of Townhouse Buildings

The minimum distance between the exterior walls of any two groups of townhouse dwellings on one lot shall be:

- | | | |
|-----|---|-------------------------|
| i) | where both ends of the group are one storey | 3 m (10 ft.) minimum; |
| ii) | in all other circumstances | 5 m (16.5 ft.) minimum. |

10.1.14 Distance Between Buildings - Apartments, Converted Buildings or Multiple Buildings

The minimum distance between any two apartment buildings, converted buildings or multiple buildings located on one lot shall be a measurement equivalent to the average height of the two buildings; however, where neither external wall facing the other building contains the window of a habitable room, the distance may be reduced to one half the average height of the two buildings.

10.1.15 Landscaped Open Space

Minimum 35% of the lot.

10.1.16 Building Height

3 storeys maximum.

10.1.17 Parking

In accordance with the provisions of Section 6.

10.1.18 Regulations for Home Occupations

Home occupations shall:

- i) be limited to one per dwelling unit;
- ii) be operated by the occupant of the dwelling unit and have no other employees than members of the family resident in the dwelling;
- iii) supply sufficient on-site parking for both the residential unit and the home occupation and in accordance with Parking Regulations, Section 6;
- iv) only have a sign that complies with the Municipality's By-law governing signs;
- v) have no external storage of materials, containers or finished products;
- vi) not change the character of the dwelling as a private residence or create or become a nuisance in regard to noise, traffic or parking;

- vii) not use any mechanical equipment, except that ordinarily used for household tasks or recreational hobbies;
- viii) not occupy more than 25% of the gross floor area of the dwelling unit or 23 m² (250 ft²) maximum including the gross floor area any basement or cellar area used as living quarters and any basement or cellar area used for the home occupation;
- ix) be confined to a dwelling unit on the lot in terms of any and all parts of the home occupation use.

10.1.19 Regulations for Accessory Buildings and Structures

Private garages and other accessory buildings and structures not attached to the main building shall:

- i) not be used for human habitation;
- ii) not be built closer to the front lot line than the main building on the lot;
- iii) have a minimum exterior side yard abutting the street line of 6 m (20 ft.) for a detached parking garage on a corner lot. For a detached accessory structure that is not intended for the storage or parking of a motor vehicle, boat storage, snowmobile, etc. and does not have an associated driveway, a minimum 1 m (3.3 ft.) exterior side yard may be permitted;
- iv) have a minimum interior side yard of 1 m (3.3 ft.). Semi-detached garages may be centred on a mutual side lot line;
- v) have a minimum rear yard of 1 m (3.3.ft.);
- vi) not exceed 4.5 m (15 ft.) in height;
- vii) not exceed 8% coverage of the lot area, provided that the coverage of all buildings on the lot shall not exceed 40 % of the lot area;
- viii) not be located within 1.5 m (5 ft.) of any other building or structure on the lot.

10.1.20 Regulations for Swimming Pools

Private swimming pools, including in-ground and above ground pools, may be constructed and maintained as accessory uses to a dwelling provided that such a use:

- i) shall only be permitted in the rear yard of the lot;
- ii) shall not be located within 1.5 m (5 ft.) of a side or rear lot line, or within 3 m (10 ft.) of a lot line which abuts the street and no water circulating or treatment equipment such as pumps or filters shall be located closer than 3 m (10 ft.) to any side or rear lot line;
- iii) shall be considered part of the landscaped open space area for the purposes of lot coverage.

10.1.21 Setback from Environmental Constraint Zone

Where any R4 Zone abuts an Environmental Constraint (EC) Zone, all buildings and structures in the R4 Zone shall be setback a minimum of 3 m (10 ft.) from the boundary of the EC Zone.

i) Density:

25 dwelling units maximum;

ii) Building Height:

The building height shall not exceed two storeys maximum at grade from Tremaine Street.

iii) Lower Level:

The lower level (east side) of the building may be used for residential dwelling units provided the minimum opening elevation to each dwelling unit is at least 79.06 m above sea level.

iv) Setback from Environmental Constraint Zone

The minimum setback from the Environmental Constraint Zone shall be zero subject to compliance with Section 10.2.16.4 iii).

SECTION 11: HIGH DENSITY RESIDENTIAL 5 (R5) ZONE REGULATIONS

11.1 GENERAL USE REGULATIONS

11.1.1 Permitted Uses

- i) public use in accordance with the provisions of Section 5.3.2;
- ii) residential use.

11.1.2 Permitted Accessory Structures

- i) accessory use to the Permitted Uses under Section 11.1.1;
- ii) one bed and breakfast establishment.

11.1.3 Permitted Buildings and Structures

- i) apartment dwelling, including apartments for senior citizens and/or the disabled;
- ii) apartment hotel dwelling;
- iii) townhouse units;
- iv) buildings and structures for public uses in accordance with the provisions of Section 5.3.2;
- v) accessory buildings and structures for the permitted uses.

11.1.4 Lot Area

- i) apartment dwelling: not applicable for apartment units;
- ii) townhouse dwelling unit: 146 m² (1,572 ft²) minimum per unit.

11.1.5 Lot Frontage

- i) apartment dwelling: 30 m (100 ft.) minimum;
- ii) townhouse dwelling unit: 6 m (19.6 ft.) minimum per unit.

11.1.6 Lot Coverage

40% maximum.

11.1.7 Net Density

- i) apartment dwelling: 100 units per net hectare (40 units per net acre) maximum.
- ii) townhouse dwelling units: 68 units per net hectare (28 units per net acre) maximum.

11.1.8 Floor Area Per Dwelling Unit

Type of Dwelling Unit	Minimum Floor Area
Bachelor Unit	28 m ² (300 ft. ²)
One Bedroom Unit	42 m ² (450 ft. ²)
Two Bedroom Unit	56 m ² (600 ft. ²)
Three Bedroom Unit	70 m ² (750 ft. ²)
Each Additional Bedroom	10 m ² (107 ft. ²)
Dwelling, Senior Citizen - Bachelor Unit	27 m ² (290 ft. ²)
Dwelling, Senior Citizen - One Bedroom Unit	40 m ² (430 ft. ²)
Dwelling, Senior Citizen - Two Bedroom Unit	48 m ² (516 ft. ²)
Dwelling, Senior Citizen - Three Bedroom Unit	62 m ² (667 ft. ²)

11.1.9 Front Yard

The established building line, or where not applicable, 6 m (20 ft.) minimum.

11.1.10 Rear Yard

7.5 m (25 ft.) minimum.

11.1.11 Interior Side Yard

- i) a townhouse unit; no interior side yard shall be required between the common vertical wall dividing one dwelling unit from another. The side yard on the other side of the unit shall be 1.5 m (5 ft.) minimum and 1 m (3.3 ft) for each additional storey;
- ii) an apartment building; 7.5 m (25 ft.) minimum.

11.1.12 Exterior Side Yard

The established building line, or where not applicable, 7.5 m (25 ft.) minimum.

11.1.13 Distance Between Buildings

The minimum distance between any two apartment dwellings on one lot shall be a measurement equivalent to the average height of the two buildings; however, where neither external wall facing the other building contains the window of a habitable room the distance may be reduced to one half the average height of the two buildings.

11.1.14 Distance Between Exterior Walls of Townhouse Dwellings

The minimum distance between the exterior walls, of any two groups of townhouse dwellings located on one lot shall be:

- i) where both ends of the group are one storey 3 m (10 ft.) minimum;
- ii) in all other circumstances 5 m (16.5 ft.) minimum.

11.1.15 Landscaped Open Space

Minimum 35% of the lot.

11.1.16 Building Height

6 storeys maximum.

11.1.17 Parking

In accordance with the provisions of Section 6.

11.1.18 Regulations for Detached Accessory Buildings and Structures

Private garages and other accessory buildings and structures not attached to the main building shall:

- i) not be used for human habitation;
- ii) not be built closer to the front lot line than the main building on the lot;
- iii) have a minimum exterior side yard abutting the street line of 6 m (20 ft.) for a detached parking garage on a corner lot. For a detached accessory structure that is not intended for the storage or parking of a motor vehicle, boat storage, snowmobile, etc. and does not have an associated driveway, a minimum 1 m (3.3 ft.) exterior side yard may be permitted;
- iv) have a minimum interior side yard of 1 m (3.3 ft.). Semi-detached garages may be centred on a mutual side lot line;
- v) have a minimum rear yard of 1 m (3.3 ft.);
- vi) not exceed 4.5 metres (15 ft.) in height;
- vii) not exceed 8 % coverage of the lot area, provided that the coverage of all buildings on the lot shall not exceed 40 % of the lot area;
- viii) not be located within 1.5 m (5 ft.) of any other building or structure on the lot.

11.1.19 Regulations for Swimming Pools

Private swimming pools, including in-ground and above ground pools, may be constructed and maintained as accessory uses to a dwelling provided that such use:

- i) shall only be permitted in the rear yard of the lot;
- ii) shall not be located within 1.5 m (5 ft.) of a side or rear lot line, or within 3 m (10 ft.) of a lot line which abuts the street and no water circulating or treatment equipment such as pumps or filters shall be located closer than 3 m (10 ft.) to any side or rear lot line;
- iii) shall be considered part of the landscaped open space area for the purposes of lot coverage.

11.1.20 Setback from Environmental Constraint Zone

Where any R5 Zone abuts an Environmental Constraint (EC) Zone, all buildings and structures in the R5 Zone shall be setback a minimum of 3 m (10 ft.) from the boundary of the EC Zone.

11.1.21 Property Abutting Railway Mainline

Where any (R5) zone abuts a railway main line:

- i) new dwelling units or other sensitive land uses (day cares, senior care facilities, emergency care facilities, etc.) within 75 metres of a railway main line will require a vibration study to determine if mitigation measures are required;
- ii) new dwelling units or other sensitive land uses (day cares, senior care facilities, emergency care facilities, etc.) within 500 metres of a railway main line will require a noise study to determine if mitigation measures are required;
- iii) new dwelling units or other sensitive land uses (day cares, senior care facilities, emergency care facilities, etc.) shall be set back from the mutual property line a minimum of 30 m (98 ft.) in conjunction with a 2.5 m (8 ft) high earthen berm. A minimum setback of 120 m (395 ft) from the mutual property line is required if a berm is not to be provided;
- iv) the earthen berm is required to have side slopes of 2.5 to 1 abutting and parallel to the mutual property line with returns at the ends;
- v) a 1.83 m (6 ft) high chain link security fence is required along the mutual property line with the railway; and
- vi) the appropriate railway will be notified of any new residential development or other new sensitive land uses for any lands that abut a railway main line requiring approval under the Planning Act R.S.O. 1990, c.P. 13, as amended.

11.1.22 Railway Yards:

- i) The minimum setback for new residential dwelling units and other new sensitive land uses (day cares, senior care facilities, emergency care facilities, etc.) from a railway yard property line is 300 m (1,000 ft). Notwithstanding, 'Lots of Record' or other infilling circumstances within the 300 m (1,000 ft.) setback from a railway yard property line which do not require a rezoning, minor variance, official plan amendment, plan of subdivision or plan of condominium approval, may be developed on an individual basis and in consultation with the appropriate railway if it has been demonstrated that the development/redevelopment meets Provincial Standards and/or Guidelines for noise and vibration and land use incompatibility, through the implementation of mitigation measures and setbacks.
- ii) Where any lands zoned (R5) are within 500 m (1,640 ft) of a railway yard property line, such lands shall be considered to be within an area of potential influence. Any new development/redevelopment of lands within this area of potential influence for residential or other sensitive land uses, will require a noise and vibration study. If it can be demonstrated that the development/redevelopment can satisfy Provincial Standards or Guidelines for noise and vibration then the development/re-development may proceed, subject to implementing any mitigation measures and setbacks required to satisfy the aforementioned Provincial Standards or Guidelines; and
- iii) The appropriate railway will be notified of any new residential development or other sensitive land uses within the 500 m area of potential influence abutting a railway yard property line.

11.1.23 Refuse and recycling enclosures

Open Storage of goods and materials are not permitted in the (R5) zone. Refuse and recycling enclosures are permitted under the following regulations:

- i) any refuse and recycling enclosures shall only be located in the side or rear yards; and
- ii) the refuse and recycling enclosure shall be constructed of an architectural concrete block, masonry, stone, stucco, and/or brick (ie: consistent with the construction of the primary building) with a minimum 1.8 m (6 ft.) in height.

11.2 SPECIAL USE REGULATIONS

The regulations contained in subsection 11.1, General Use Regulations shall apply to the special use area or areas defined below, except as otherwise specifically provided in the special use regulations in this subsection 11.2.

11.2.1 Apartment Residential 5 Exception 1 (R5-1) - 434 William Street

11.2.1.1 Defined Area

R5-1 as shown on Schedule "A", Map 7 to this By-law.

11.2.1.2 Permitted Uses, Buildings and Structures

The uses, buildings and structures permitted in Section 11.1

11.2.1.3 Regulations for the Uses Permitted in the R5-1 Zone

The regulations of Section 11.1 shall apply to the uses permitted in the R5-1 Zone, except that the maximum building height shall be 7 storeys, and the buildings and structures shall be limited to those existing at the date of adoption of this By-law.

11.2.2 Apartment Residential 5 Exception 2 (R5-2) - Legion Village

11.2.2.1 Defined Area

R5-2 as shown on Schedule "A", Map 2 to this By-law.

11.2.2.2 Permitted Uses, Buildings and Structures

The uses, buildings and structures permitted in Section 11.1 and a hostel, including uses accessory to such use.

11.2.2.3 Regulations for the Uses Permitted in the R5-2 Zone

The regulations of Section 11.1 shall apply to the uses permitted in the R5-2 Zone, except that:

- i) the maximum number of dwelling units shall be 172 units for the elderly and disabled and 32 hostel dwelling units with no more than 2 beds per unit;
- ii) one additional unit shall be permitted for the building manager of each permitted building;
- iii) the maximum height shall be 4 storeys, except for a hostel building which shall have a maximum height of 1 storey, and furthermore, the buildings and structures shall be limited to those existing at the date of adoption of this By-law, and landscaped open space shall not be reduced below the amount existing at the date of adoption of this By-law;