

# PLANNING JUSTIFICATION REPORT

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*KERR STREET SUBDIVISION  
KWENDILL HOLDINGS LTD.  
TOWN OF COBOURG*



Submitted By:

**RFA Planning Consultant Inc.**

202-211 Dundas Street East, Belleville, Ontario, K8N 1E2

MARCH, 2021

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## 1. INTRODUCTION

### 1.1 The Kerr Street Subdivision Project

RFA Planning Consultant Inc. was retained by Kwendill Holdings Ltd., to undertake professional planning services for the Kerr Street subdivision project. An Application for Approval of a Plan of Subdivision has been filed concurrently with the Town of Cobourg. This Planning Justification Report has been prepared in support of this application. The Draft Plan of Subdivision proposes to create six (6) lots for general industrial use, along with blocks for shared, private storm water management and blocks for shared access.

The land is bounded to the south by a Canadian National Railway (CNR) main line, and along the north by Kerr Street. The subject lands are currently vacant, but consists of a CNR spur line traversing the western property line by easement (Part 7, Plan 39R-8306), which extends north into the Northam Industrial Park from the CNR mail line.

### 1.2 Site Location & Context

#### **Legal Description:**

The land is legally described as Part of Park Lot 'L', Part of Wellington Street (closed) (formerly Part of Lot 16, Concession A, Township of Hamilton), Part of the Glebe Lot and Part of Lots 1, 2 and 3 Block 'K', Caddy Plan (formerly Part of Lot 15, Concession A, Township of Hamilton), Being Parts 6, 7 and 8, Plan 39R-8306 except Part 1, Plan 39R-13906, Town of Cobourg, County of Northumberland.

#### **Site Area and Frontage:**

The subdivision is approximately 6.21 hectares (15.35 acres) in area and has 389.8 metres (1,278.7 feet) of frontage along the south side of Kerr Street.

#### **Surrounding Land Uses:**

The surrounding land uses are indicated below:

To the east: Limpact International Ltd. (GM Zone);

To the west: Home Hardware and CNR spur line (GM Zone);

To the north: Northam Industrial Park – nearest are CCS Industrials, National Shunt Service (GM Zone);

To the south: CNR main line (TC Zone) with Industrial, High-Density Residential and Shopping Centre further south.

Figure 1 – Location Map below shows the location of the subject property on an aerial image.

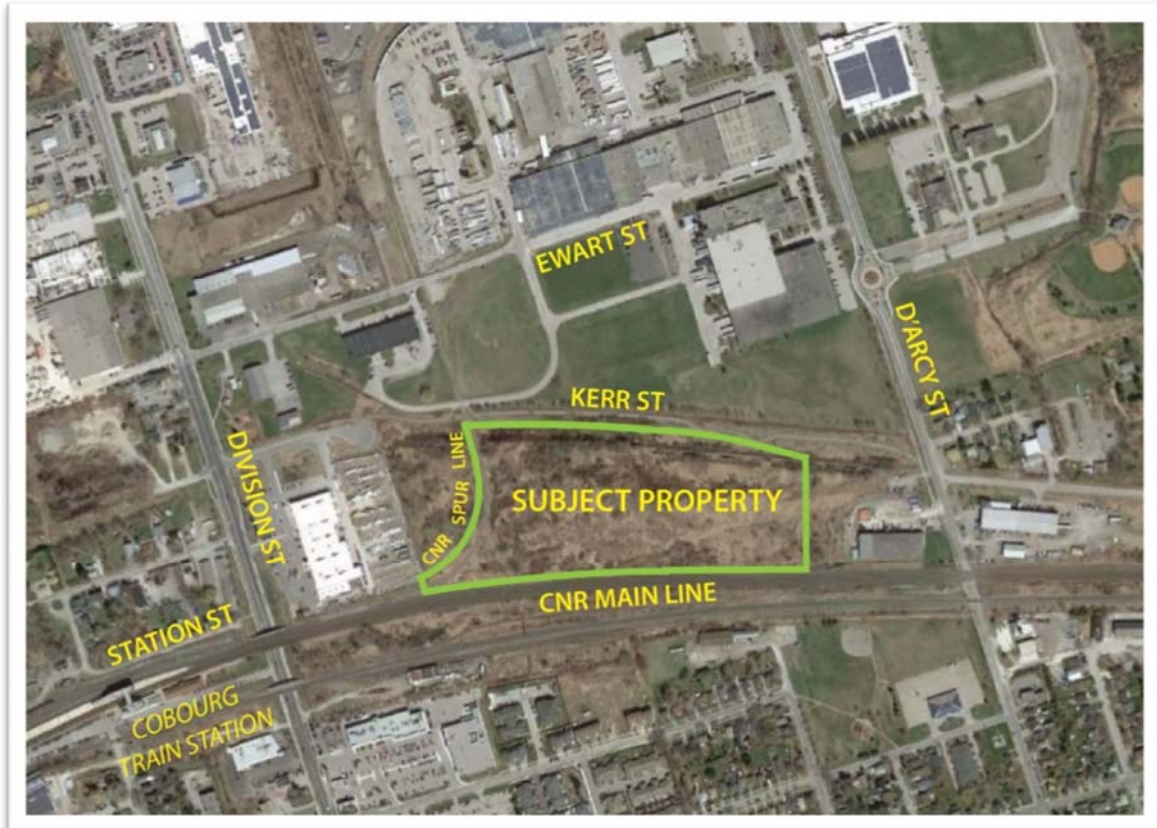


Figure 1: Location Map – Subject property outlined in green.



**Lot 1** – View looking south-east from Kerr Street multi-use path; CNR spur line crossing (centre).



**Northam Industrial Park** – View looking north from Kerr Street multi-use path; CNR spur line (centre).



**Lot 1** – View looking south along Lot 1 west property line; CNR spur line (centre).



**Lot 1** – View of Lot 1 west and south property lines looking south; Victoria Hall (centre).



**Northam Industrial Park** – View of streetscape looking north-east from Kerr multi-use path.



**Lot 2** – View looking south-west from Kerr Street; West Street intersection (centre).



**Lot 6** – View of looking south from the Kerr Street multi-use path.



**Limpact International Limited** – View of looking south from the Kerr Street multi-use path.



## 2. PROPOSED DRAFT PLAN

**Figure 2 – Draft Plan of Subdivision** dated March 16, 2021, prepared by RFA Planning Consultant Inc., is the subject of the application to the Town of Cobourg. The draft plan illustrates the proposed lot configuration, along with the proposed shared access blocks and shared storm water management blocks.

**Figure 3 – Development Site Plan** dated February 16, 2021, prepared by RFA Planning Consultant Inc., illustrates potential building footprints, parking and access arrangements along with the configuration of the required 2.5-metre high earthen safety berm, fencing and storm water facility design.

**Table 1** summarizes the land use of the subdivision as follows:

**Table 1: Land Use Summary**

Land Use	Area (ha)	Area (%)	Units
<b>Lots 1-6</b>	4.24 ha	68.2 %	6
<b>Blocks 7-11 Access Easement</b>	0.49 ha	8.0 %	
<b>Blocks 12-17 Storm Water Easement</b>	1.22 ha	19.7 %	
<b>Block 18 CNR Spur Line</b>	0.25 ha	4.1 %	
<b>Site Area Total</b>	6.21 ha	100.0 %	6
<b>Overall Subdivision Density (net) – 2 units per net hectare</b>			

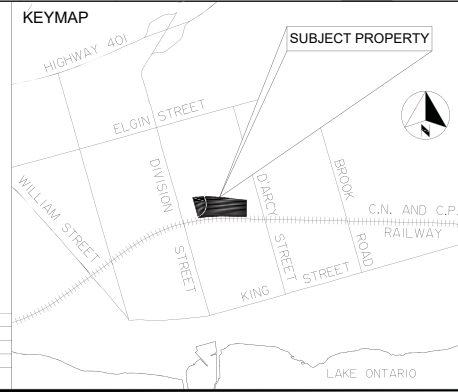
- The immediate portion of Kerr Street that the subject property fronts onto has been newly constructed and is now open along the subject property as a public road maintained year-round from Division Street to D’Arcy Street. Kerr Street will provide access to all the subdivided lots.
- There will be access easements along the northern extent of Lots 2, 3, 4, 5 and 6. This is necessary due to a future median planned along Kerr Street that will restrict access into the subdivision once Kerr Street is fully constructed. Prior to the full build-out of Kerr Street, Lots 1 to 6 will have full-movement access.

- There will be storm water easements totaling 1.2 hectares along the southern portion of the property for the construction and maintenance of a shared storm water facility. The easement will extend 30 metres north from the southern extent of the lot. Storm water management will be shared amongst all lots to address quantity control only. The storm water easement will consist of a storm water facility, access road, and earthen berm.
- A multi-purpose trail has been included with the recently constructed portion of Kerr Street – between Division Street and D’Arcy Street – to facilitate active transportation.
- The subdivision will be serviced by existing municipal services.
- All of the lots abut the CNR main line and a 2.5-metre high earthen safety berm is recommended along the south property line. A 1.83-metre high chain-link security fence is required along the mutual property line with the railway. Lots 2-6 will consist of a 15-metre building setback with a 2.5-metre-high earthen safety berm. Due to an existing box culvert interrupting the safety berm design, it will not cover the entirety of Lot 1. Lot 1 will consist of a 60-metre building setback if the building will be used for employment purposes. This setback can be reduced to 15 metres if the building is not being used for employment (i.e., warehousing, etc.).



**DRAFT PLAN OF SUBDIVISION**  
 PART OF PARK LOT 'L'  
 PART OF WELLINGTON STREET (CLOSED)  
 (FORMERLY PART OF LOT 16, CONCESSION A, TOWNSHIP OF HAMILTON)  
 PART OF THE GLEBE LOT AND PART OF LOTS 1, 2 AND 3 BLOCK 'K'  
 CADDY PLAN  
 (FORMERLY PART OF LOT 15, CONCESSION A, TOWNSHIP OF HAMILTON)  
 BEING PARTS 6, 7 & 8, PLAN 39R-8306, EXCEPT PART 1, PLAN 39R-13906  
**TOWN OF COBOURG**  
 COUNTY OF NORTHUMBERLAND  
 SCALE = 1:750 METRIC

WEST AND CENTRE STREET EXTENSIONS  
 REVISION  
 DATE  
 SCALE: 1:500



**LAND USE SCHEDULE**

LAND USE	AREA(m <sup>2</sup> )	AREA%	UNITS
LOTS 1-6	42,362.5	68.2	6
BLOCKS 7-11 ACCESS EASEMENT	4,938.5	8.0	
BLOCKS 12-17 STORM WATER EASEMENT	12,242.4	19.7	
BLOCK 18 CNR SPUR LINE	2,546.9	4.1	
<b>TOTAL</b>	<b>62,107.1m<sup>2</sup></b>	<b>100.0%</b>	<b>6</b>

**ADDITIONAL INFORMATION REQUIRED UNDER SECTION 51(17) OF THE PLANNING ACT.**

- (a) SEE SURVEYORS CERTIFICATE.
- (b) AS SHOWN ON DRAFT PLAN.
- (c) AS SHOWN ON DRAFT PLAN.
- (d) SEE LAND USE SUMMARY
- (e) SEE DRAFT PLAN
- (f) AS SHOWN ON DRAFT PLAN.
- (g) AS SHOWN ON DRAFT PLAN.
- (h) MUNICIPAL WATER AND SANITARY SEWER
- (i) SANDY SILT
- (j) AS SHOWN ON DRAFT PLAN.
- (k) GARBAGE COLLECTION, FIRE PROTECTION, ROAD MAINTENANCE
- (l) AS SHOWN ON DRAFT PLAN.

**METRIC NOTE:**  
 DISTANCES SHOWN ON THIS PLAN ARE IN METRES AND CAN BE CONVERTED TO BY DIVIDING BY 0.3048.

**TOPOGRAPHY NOTE:**  
 EXISTING SPOT ELEVATIONS PREPARED BY DOBRI ENGINEERING LTD. CONTOURS DRAWN AT INTERVALS OF 1.0m.

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**SURVEYOR'S CERTIFICATE**  
 I CERTIFY THAT THE BOUNDARIES OF THE LAND TO BE SUBDIVIDED ARE CORRECTLY SHOWN.

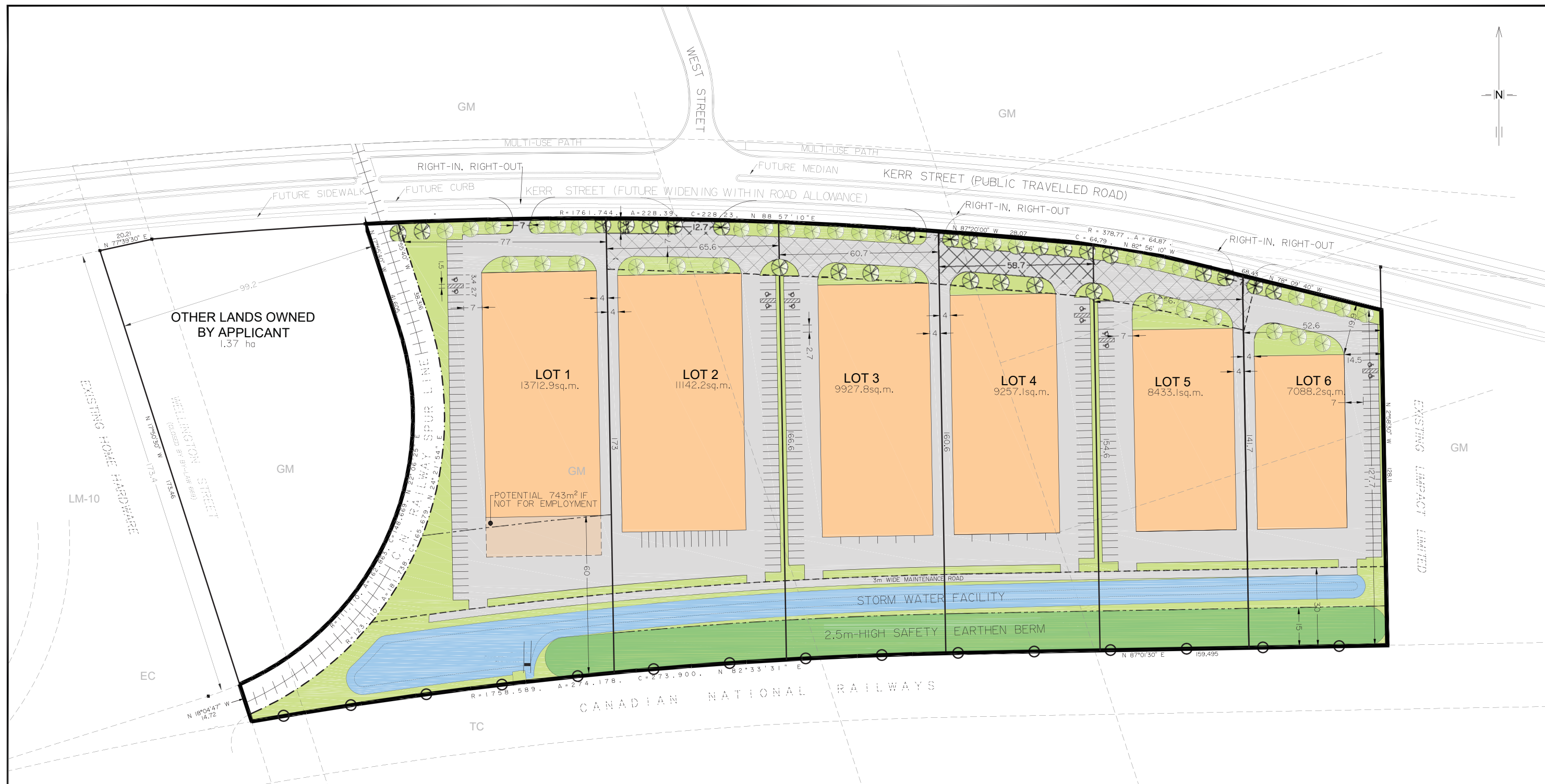
*Merrill D. McLean* March 16, 2021  
 MERRILL D. McLEAN DATE  
 ONTARIO LAND SURVEYOR.

**SYLVESTER & BROWN**  
 191 TORONTO ROAD, PORT HOPE ONTARIO, L1A 3V5  
 (905) 885-2260

**MARCH 16, 2021**

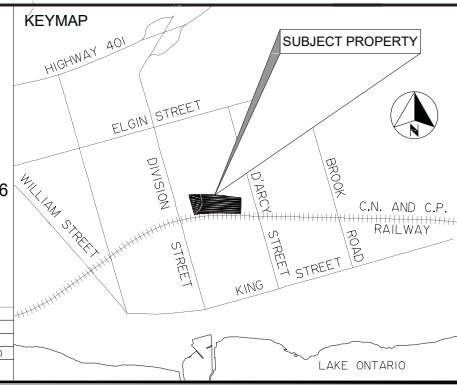
JOB No. **546 - CP**

**RFA**  
 PLANNING CONSULTANT INC.  
 211 Dundas Street East, Suite 202,  
 Belleville, Ontario, K8N 1E2



**DEVELOPMENT SITE PLAN**  
 PART OF PARK LOT 'L'  
 PART OF WELLINGTON STREET (CLOSED)  
 (FORMERLY PART OF LOT 16, CONCESSION A, TOWNSHIP OF HAMILTON)  
 PART OF THE GLEBE LOT AND PART OF LOTS 1, 2 AND 3 BLOCK 'K'  
 CADDY PLAN  
 (FORMERLY PART OF LOT 15, CONCESSION A, TOWNSHIP OF HAMILTON)  
 BEING PARTS 6, 7 & 8, PLAN 39R-8306, EXCEPT PART 1, PLAN 39-13906  
 TOWN OF COBURG  
 COUNTY OF NORTHUMBERLAND  
 SCALE = 1:1500 METRIC

REVISION: \_\_\_\_\_ DATE: \_\_\_\_\_ APPRD: \_\_\_\_\_  
 DRAWN BY: L.B.A.C. CHECKED BY: S.L. DATE: FEBRUARY 16, 2021 SCALES: 1:500



- LEGEND**
- LANDSCAPED OPEN SPACE
  - INDUSTRIAL BUILDING FOOTPRINT
  - ACCESS EASEMENT IN FAVOUR OF LOTS 1, 3, 4, 5, 6
  - STREET TREES
  - 15m or 60m SETBACK FROM C.N. MAIN LINE
  - NORTHERN LIMIT OF 30m STORM WATER EASEMENT IN FAVOUR OF LOT 1-6
  - EXISTING CNR SPUR EASEMENT
  - 1.83-HIGH CHAIN-LINK FENCE

**ZONING SUMMARY**  
 ZONING BY-LAW NO #85-2003

PROVISION	REQ'D	LOT 1	LOT 2	LOT 3	LOT 4	LOT 5	LOT 6
LOT AREA (min)	1,400m <sup>2</sup>	13,713m <sup>2</sup>	11,142m <sup>2</sup>	9,928m <sup>2</sup>	9,257m <sup>2</sup>	8,433m <sup>2</sup>	7,088m <sup>2</sup>
LOT FRONTAGE (min)	30m	77m	65.6m	60.7m	58.7m	56.7m	52.6m
LOT COVERAGE (max)	60%	35%	42%	41%	40%	35%	32%
FRONT YARD (min)	9m	20m	20m	20m	20m	20.1m	19.9m
REAR YARD (min)	7.5m	43.1m	43m	43m	43m	30m	30m
INTERIOR SIDE YARD (min)	3m	4m	4m	4m	4m	4m	4m
EXTERIOR SIDE YARD (min)	9m	N/A	N/A	N/A	N/A	N/A	N/A
LANDSCAPED OPEN SPACE (min)	10%	24.9%	19.1%	20.1%	21.7%	24.5%	27.5%
BUILDING HEIGHT (max)	13m	13m	13m	13m	13m	13m	13m
PARKING AND LOADING (min) PER 100m <sup>2</sup> GFA	1.0	1.1	1.2	1.1	1.1	1.2	1.4

**METRIC NOTE:**  
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**BUILDING NOTE:**  
 INDUSTRIAL BUILDING FOOTPRINTS, LANDSCAPED OPEN SPACE AND PARKING AREAS ARE CONCEPTUAL AND SUBJECT TO CHANGE.

FEBRUARY 16, 2021

JOB No. 546 - CP

211 Dundas Street East, Suite 202, Belleville, Ontario, K8N 1E2

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### 3. TECHNICAL SUPPORT STUDIES FOR THE KERR STREET SUBDIVISION

Related technical support studies have been prepared to address the following components of the Kerr Street subdivision project:

- Tree Inventory & Assessment prepared by Cressman Tree Maintenance & Landscaping Ltd., dated January 18, 2021;
- Geotechnical Investigation Report prepared by GHD Limited, dated January 15, 2021;
- Functional Servicing Study prepared by Dobri Engineering Ltd., dated February 18, 2021;
- Stormwater Management Report prepared by Dobri Engineering Ltd., dated February 18, 2021.

These reports have been submitted under separate covers with a preceding consent application. PDF copies have been provided with the subject plan of subdivision application package. The following is a synopsis of the reports and recommendations.

#### 3.1 Tree Inventory & Assessment

A Tree Inventory & Assessment was prepared by Cressman Tree Maintenance & Landscaping Ltd. A field investigation was undertaken and a report was prepared January 18, 2021. Through this field investigation, Cressman undertook a complete walk-through of the entire property and plotted the approximate location of groups of trees. The findings concluded that there are no species of great value or any endangered species on the property. It is the opinion of Del Cressman B.C.M.A, President of Cressman Tree Maintenance & Landscaping Ltd., that there would be little value in preserving any of the trees based on the species being predominately Popular, Manitoba Maple, Black Willow, and Black Locust which are all generally in fair to poor condition.

It is understood that a tree replacement plan/Landscape Plan may be a requirement of Site Plan Approval on a per lot basis, to which this report will form the basis.

#### 3.2 Geotechnical Investigation Report

GHD Limited prepared a Geotechnical Investigation Report dated January 15, 2021. The purpose of the GHD's investigation was to explore the subsurface soil and groundwater conditions at the project site, and provide the accompanying geotechnical engineering recommendations.

A field investigation was conducted on December 22, 2020. Twelve exploratory test holes were logged and sampled. Soil samples were inspected immediately in the field, and physical laboratory analysis was carried out.

From a topographical standpoint, the high point is in the east boundary and a low point is in the south-west corner of Lot 1, where a culvert crosses the rail spur line. Based on the information gathered during this investigation, the site's subsurface stratigraphy generally consists of surficial layer of topsoil followed by native sandy silt or clayey silt and/or glacial till.

The final recommendations from GHD include excavation and construction practices being carried out following the Occupational Health and Safety Act guidelines for Type 2 and 3 soil. The soil in the middle of the property will require further testing to determine footing depth and design. Below the topsoil, the stiff or compact till would be suitable for founding the building foundations. Under no circumstances should the foundations be placed above fill, organic materials, loose, frozen subgrade, construction debris, or within ponded water.

All surficial topsoil, organics, organic-bearing materials, disturbed soils, and other deleterious materials from the planned access road and parking lot areas should be removed. Stringent construction control procedures should be maintained to ensure that uniform subgrade moisture and density conditions are achieved as much as possible regarding the pavement structures.

A storm water management facility has been proposed at the southerly boundary of the site. Given the low hydraulic conductivity of the site soils there was no effective infiltration and thus no need for a liner in the pond area.

In general, care must be taken to protect the exposed subgrade from excess moisture and construction traffic. Exposed surfaces intended to support foundations and slabs must be protected against freezing. Finally, geotechnical inspection and review of foundation excavations and compaction procedures must be carried out to ensure compliance with GHD's recommendations.

### 3.3 Functional Servicing Study

Dobri Engineering Ltd. prepared a Functional Servicing Study. Dobri confirmed that the new Kerr Street construction included the following municipal services:

- A new storm sewer system, that was not designed to accept any runoff from this development.
- 250mm sanitary sewer outletting into the existing sanitary sewer on D'Arcy Street. The sanitary sewer does not extend the full length of the property. Only the eastern portion can be serviced via the new sanitary gravity sewer. Lots 1 and 2...

- The existing 450mm watermain within the Kerr Street extension has been tapped and four fire hydrants have been installed on the south side of Kerr Street, in front of the development land.
- Hydro/Bell lines have been relocated.
- Natural gas is not presently available, but is planned to be extended to the proposed new lots as they develop.

Once Kerr Street is fully constructed, westbound access will be limited, due to the proposed median. A full-movement entrance will align with West Street on Lot 2, and will be shared by Lots 1, 3, 4, 5 and 6. Once Kerr Street is fully constructed, direct access to Lots 1, 3, 4, 5 and 6 will be limited to right-in, right-out.

Sanitary sewer capacity downstream of the proposed development was monitored for three months, from April 2, 2020 to July 8, 2020. The conclusion is that the existing sanitary sewer has the capacity for the Kerr Street subdivision.

### 3.4 Stormwater Management Report

A Stormwater Management Report was prepared by Dobri Engineering Ltd. Dobri confirms the Kerr Street subdivision is within the Mid-Town Creek catchment area but that the storm water system on the newly constructed portion of Kerr Street – between Division Street and D’Arcy Street – was not designed to accept any runoff from this development. Surface runoff on the subject property is currently directed towards an existing box culvert crossing the CNR main line, near the south-west corner of the subdivision area. Dobri also confirms that the newly constructed Kerr Street extension now intercepts the surface runoff from the Northam Industrial Park, reducing the flow through the box culvert, which forms the basis of their storm water management plan.

A permanent storm water quantity control facility will be provided at the southern extent of the subdivision area. The peak flow for the 2-year storm event will be controlled to 50% of the peak pre-development flow and the 5-year and 100-year storm events will be controlled to 70% of their respective peak pre-development flows. The facility on the subdivision lands has been designed to also accept discharge from the “Other Lands Owned by the Applicant” (Proposed Severed Parcel). Discharge is to be controlled by a weir structure outside of the exiting box culvert. A 3.0-metre wide access road will be constructed to connect each individual lot to the storm facility for required maintenance. Dobri also recommends at this time a 3-metre wide grassed filter strip on the north side of the access road to address permanent quality control. This, and other quality control measures (i.e., side yard grassed swales) will need to be evaluated during site plan approval process. Quality control is to be provided and be subject to Site Plan Control on a per lot basis. Dobri also provides erosion and sedimentation control measures for both before and during construction.

## 4. PROVINCIAL POLICY STATEMENT (2020) ANALYSIS

The Provincial Policy Statement (PPS) applies to all planning applications effective May 1, 2020. It provides policy direction on matters of provincial interest related to land use planning and development. All decisions related to land use planning matters “shall be consistent with” the PPS. The subject Application for Approval of a Plan of Subdivision for Kwendill Holdings Ltd. is consistent with the PPS.

Table 2 demonstrates how the Kerr Street subdivision is consistent with the Provincial Policy Statement.

Table 2: Provincial Policy Statement and Analysis

PPS POLICIES	PLANNING ANALYSIS
<b>1 BUILDING STRONG HEALTHY COMMUNITIES</b>	
<b>1.1 MANAGING AND DIRECTING LAND USE TO ACHIEVE EFFICIENT AND RESILIENT DEVELOPMENT AND LAND USE PATTERNS</b>	
<b>1.1.3 Settlement Areas</b>	
<i>1.1.3.1 Settlement areas shall be the focus of growth and development.</i>	The subject property is within the Town of Cobourg built boundary, a designated settlement area within the Growth Plan.
<p><i>1.1.3.2 Land use patterns within settlement areas, land use patterns shall be based on densities and a mix of land uses which:</i></p> <ul style="list-style-type: none"> <li><i>a) efficiently use land and resources;</i></li> <li><i>b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;</i></li> <li><i>c) minimize negative impacts to air quality and climate change, and promote energy efficiency;</i></li> <li><i>e) support active transportation;</i></li> <li><i>f) are transit-supportive, where transit is planned, exists or may be developed; and</i></li> <li><i>g) are freight-supportive.</i></li> </ul>	Division of the subject property is considered a form of infill, or vacant-lot, intensification, being within the built boundary and surrounded by existing development. The site can be serviced by municipal water and sanitary systems, which are now available as part of the Kerr Street extension project. Specifically, only the eastern portion (Lots 3 to 6) of the Kerr Street subdivision can be serviced via the new sanitary gravity sewer. The balance (Lots 1 and 2) of the subdivision will require a sanitary force main. On this basis, land and resources are being used efficiently along with existing infrastructure and public service facilities. Any proposed facility will be subject to current Ontario Building Code and Ministry of the Environment, Conservation and Parks (MOECP) regulations. On this basis, negative impacts to air quality and climate change are anticipated to be



PPS POLICIES	PLANNING ANALYSIS
	<p>mitigated within regulatory standards. The proposed development is along Kerr Street, which, in addition to the new multi-use path, will also include new sidewalks when the extension is complete, facilitating active transportation. The Kerr Street subdivision is approximately 485 metres east of the Cobourg train station. Also, it is presumed that Kerr Street, as an arterial will be a candidate for public transit. A CNR spur line traverses Lot 1. On this basis, the proposed development will be transit and freight supportive.</p>
<b>1.3 EMPLOYMENT</b>	
<p><i>1.3.1 Planning authorities shall promote economic development and competitiveness by:</i></p> <p><i>b) providing opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses;</i></p> <p><i>e) ensuring the necessary infrastructure is provided to support current and projected needs.</i></p>	<p>b) The proposed development will contribute to a range of suitable sites for employment uses. The subject property, in its current form, has been vacant for decades and the proposed division of land is part of a plan to provide a range and choice of suitable sites for employment uses and contribute to a wide range of economic activities.</p> <p>e) The proposed development can have full municipal services, which are now available as part of the Kerr Street extension project.</p>
<b>1.3.2 Employment Areas</b>	
<p><i>1.3.2.1 Planning authorities shall plan for, protect and preserve employment areas for current and future uses and ensure that the necessary infrastructure is provided to support current and projected needs.</i></p>	<p>No change in Official Plan land use designation or zoning is proposed, which will protect and preserve an existing employment area. The current Employment Area Official Plan designation and General Industrial (GM) Zone are to remain. It is proposed to subdivide the land only.</p>

PPS POLICIES	PLANNING ANALYSIS
<p><i>1.3.2.3 Within employment areas planned for industrial or manufacturing uses, planning authorities shall prohibit residential uses and prohibit or limit sensitive land uses that are not ancillary to the primary employment uses in order to maintain land use compatibility.</i></p> <p><i>Employment areas planned for industrial or manufacturing uses should include an appropriate transition to adjacent non-employment areas.</i></p>	<p>Only accessory single-detached units for caretakers are permitted as-of-right within the current GM Zone. This development potential is intended to be retained.</p>
<p><i>1.3.2.6 Planning authorities shall protect employment areas in proximity to major goods movement facilities and corridors for employment uses that require those locations.</i></p>	<p>Lot 1 will be “subject to” and “together with” the CNR spur line, respectively, in order to preserve its function for existing and future employment uses that require proximity to a major goods movement corridor.</p>
<b>1.6 INFRASTRUCTURE AND PUBLIC SERVICE FACILITIES</b>	
<b>1.6.6 Sewage, Water and Stormwater</b>	
<p><i>1.6.6.2 Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas to support protection of the environment and minimize potential risks to human health and safety. Within settlement areas with existing municipal sewage services and municipal water services, intensification and redevelopment shall be promoted wherever feasible to optimize the use of the services.</i></p>	<p>The site can be serviced by municipal sewage and water and the proposed development will optimize its use.</p>
<b>1.6.7 Transportation Systems</b>	
<p><i>1.6.7.2 Efficient use should be made of existing and planned infrastructure, including through the use of transportation demand management strategies, where feasible.</i></p>	<p>Proximity of the subject property to the train station (485 metres), along with the new Kerr Street multi-use path, will facilitate minimizing the length and number of vehicle trips as the public can make use of existing infrastructure.</p>

PPS POLICIES	PLANNING ANALYSIS
<b>1.6.8 Transportation and Infrastructure Corridors</b>	
<p><i>1.6.8.1 Planning authorities shall plan for and protect corridors and rights-of-way for infrastructure, including transportation, transit and electricity generation facilities and transmission systems to meet current and projected needs.</i></p>	<p>It is proposed to protect the CNR spur line right-of-way (easement) for existing and future employment uses on Lot 1 and the employment lands within the Northam Industrial Park, respectively.</p>
<p><i>1.6.8.3 Planning authorities shall not permit development in planned corridors that could preclude or negatively affect the use of the corridor for the purpose(s) for which it was identified.</i></p> <p><i>New development proposed on adjacent lands to existing or planned corridors and transportation facilities should be compatible with, and supportive of, the long-term purposes of the corridor and should be designed to avoid, mitigate or minimize negative impacts on and from the corridor and transportation facilities.</i></p>	<p>The Kerr Street subdivision is within a designated employment area that abuts an existing CNR transportation corridor. These uses are considered compatible with and supportive of the long-term purposes of the corridor. There are no apparent negative impacts from prospective employment uses on the CNR corridor.</p>
<b>1.6.9 Airports, Rail and Marine Facilities</b>	
<p><i>1.6.9.1 Planning for land uses in the vicinity of airports, rail facilities and marine facilities shall be undertaken so that:</i></p> <p><i>a) their long-term operation and economic role is protected;</i></p>	<p>The prospective employment uses are anticipated to support the nearby rail station, 485 metres to the west.</p>

## 5. CONFORMITY TO THE GROWTH PLAN (2019)

A Place to Grow: Growth plan for the Greater Golden Horseshoe (Growth Plan) has been applied to all planning applications since May, 2019. It provides policy direction on matters related to land use planning and development within the Greater Golden Horseshoe. All decisions related to land use planning matters “shall be consistent with” the Growth Plan. In reviewing the 2019 Growth Plan, it was found that the intent of the relevant policies has been maintained and the application for Kwendill Holdings Ltd. are consistent with the Plan.

Table 3, below, is an analysis of the applicable Growth Plan policies.

Table 3: Growth Plan Analysis

GROWTH PLAN POLICIES	PLANNING ANALYSIS
<b>2 WHERE AND HOW TO GROW</b>	
<b>2.2 POLICIES FOR WHERE AND HOW TO GROW</b>	
<b>2.2.1. Managing Growth</b>	
<p><i>2. Forecasted growth to the horizon of this Plan will be allocated based on the following:</i></p> <p><i>a) the vast majority of growth will be directed to settlement areas that:</i></p> <ul style="list-style-type: none"> <li><i>i) have a delineated built boundary</i></li> <li><i>ii) have existing or planned municipal water and wastewater systems;</i></li> <li><i>iii) can support the achievement of complete communities</i></li> </ul>	<p>The subject property is within the delineated Town of Cobourg “Urban Settlement Area” and “Built Boundary”. The proposed development will have access to municipal water and wastewater systems, which are now available.</p> <p>Complete communities are “compact, transit-supportive, and make effective use of investments in infrastructure and public service facilities” (Section 1.2). The subject property fronts onto Kerr Street, which is undergoing a multi-phase extension. A two-lane roadway, multi-use path and municipal water and wastewater mains have been recently constructed. As Kerr Street is an arterial servicing an employment area, it is presumed to be a candidate for transit. Lot 1 will also maintain the use of the CNR spur line (existing infrastructure) and the proposed development overall may benefit from its proximity to the rail station 485</p>

GROWTH PLAN POLICIES	PLANNING ANALYSIS
<b>2 WHERE AND HOW TO GROW</b>	
<b>2.2 POLICIES FOR WHERE AND HOW TO GROW</b>	
	metres to the west (public service facilities). These features will support the achievement of a complete community.
<b>2.2.2 Delineated Built-up Areas</b>	
<p><i>3. All municipalities will develop a strategy to achieve the minimum intensification target and intensification throughout delineated built-up areas, which will:</i></p> <ul style="list-style-type: none"> <li><i>a) identify strategic growth areas to support achievement of the intensification target and recognize them as a key focus for development;</i></li> <li><i>b) identify the appropriate type and scale of development in strategic growth areas and transition of built form to adjacent areas;</i></li> <li><i>c) encourage intensification generally throughout the delineated built-up area;</i></li> <li><i>d) ensure lands are zoned and development is designed in a manner that supports the achievement of complete communities;</i></li> <li><i>e) prioritize planning and investment in infrastructure and public service facilities that will support intensification; and</i></li> <li><i>f) be implemented through official plan policies and designations, updated zoning and other supporting documents.</i></li> </ul>	<p>The Town of Cobourg has an intensification target, which will be further analyzed below. The proposed development is considered infill/vacant-lot within the “Built Boundary”. The subject lands are also in close proximity to a train station (485 metres), along with abutting a major transit station area, where intensification is encouraged. It is important to note that the Town’s investment in infrastructure (i.e., the Kerr Street extension) is acting as a catalyst to facilitate intensification of the subject property.</p>

## 6. CONFORMITY TO THE COUNTY OF NORTHUMBERLAND OFFICIAL PLAN (2016)

The County of Northumberland Official Plan was approved by the OMB on November 23, 2016. The lands are designated “Urban Area” on Schedule ‘A’ – Land Use Plan of the County of Northumberland Official Plan. A full range of uses are permitted with Urban Areas (C1.4). The Application for Approval of a Plan for Subdivision conforms to the policies of the County Official Plan. The policies of the County Official Plan have incorporated relevant policies of the Growth Plan.

**Legend**

**Land Use Designations**

-  Urban Area
-  Rural Settlement Area
-  Major Employment Area
-  Oak Ridges Moraine Settlement Area
-  Rural Area
-  Agricultural Area
-  Environmental Protection Area

**Transportation Elements**

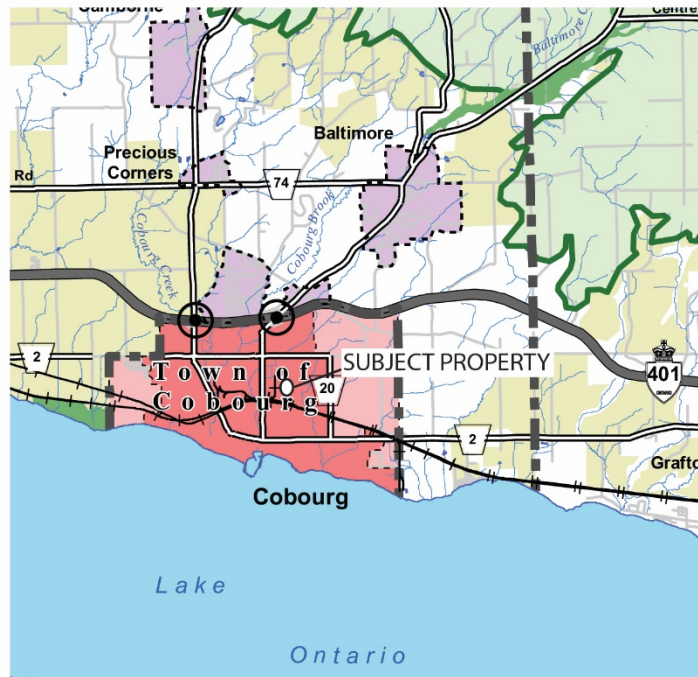
-  Highway 401
-  County Road/Arterial Road
-  Local Road
-  Highway 401 Interchange
-  Railway

**First Nations Lands**

-  Alderville

**Other Map Elements**

-  Built Boundary
-  Provincial Park
-  Oak Ridges Moraine Conservation Plan Boundary
-  Trent Severn Waterway
-  Northumberland Boundary
-  Municipal Boundary



**Figure 4: Schedule A Land Use Extract to County of Northumberland Official Plan**

Sections B, C and E that provide policies applying to subdivisions and employment development within urban areas have been analyzed in **Table 4** below.

**Table 4: County of Northumberland Official Plan Analysis**

COUNTY OF NORTHUMBERLAND OFFICIAL PLAN POLICIES	PLANNING ANALYSIS
<b>B GROWTH MANAGEMENT</b>	
<b>B1 URBAN AREAS/RURAL SETTLEMENT AREAS</b>	
<p><i>Urban areas and rural settlement areas shall be the focus of growth and their vitality and regeneration shall be promoted.</i></p>	<p>The proposed development is located within the Cobourg Urban Area as designated on Schedule A Land Use of the County of Northumberland Official Plan, which shall be the focus of growth.</p>
<b>B9 MINIMUM INTENSIFICATION TARGET</b>	
<p><i>The Cobourg Urban Area has a minimum intensification target of 39% of the projected population growth for the County of Northumberland in the form of intensification (Table 1 Minimum Intensification Target in Planning Period).</i></p>	<p>The proposed development will assist the Cobourg Urban Area in achieving their minimum intensification target.</p>
<b>B13 PLANNING FOR EMPLOYMENT</b>	
<p><i>a) Ensuring that there is employment land in appropriate locations to attract the widest range of possible uses is a key objective of this Official Plan...</i></p>	<p>The subject lands are located within a designated Employment Area. The proposed subdivision will continue to facilitate a range of possible uses within the GM Zone on the subject lands, which is to remain.</p>
<b>C LAND USE DESIGNATIONS</b>	
<b>C1 URBAN AREAS / RURAL SETTLEMENT AREAS</b>	
<b>C1.1 Complete Communities</b>	
<p><i>The County encourages each of the six urban areas to become complete communities where there is:</i></p> <p><i>a) A strong live/work ratio, where the majority of residents are employed in the community in which they live;</i></p>	<p>The subject property is within a greater employment area located centrally within the Town of Cobourg. Existing residential neighbourhoods and commercial nodes, along with direct access to Cobourg's arterial road</p>

COUNTY OF NORTHUMBERLAND OFFICIAL PLAN POLICIES	PLANNING ANALYSIS
<p><i>b) A population level and density that supports the provision of public transit, where feasible</i></p>	<p>network and proximity to a major rail transit station surround this central employment area. These features are conducive to a strong live/work ratio. Intensification along Kerr Street, an arterial, may be transit supportive as it builds out, making it a potential candidate for public transit.</p>
<p><b>C1.2 General Land Use Objectives for Urban Areas</b></p>	
<p><b>C1.2.3 Employment Areas and Uses</b></p>	
<p><i>It is the objective of this Plan to:</i></p> <p><i>b) Recognize that there are many types of employment in the County and that each has different locational requirements that are necessary to support their continued viability;</i></p> <p><i>c) Provide for an appropriate mix and range of employment and institutional uses to meet long-term needs;</i></p> <p><i>d) Provide opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses;</i></p> <p><i>f) Ensure that the necessary infrastructure is provided to support current and projected needs;</i></p> <p><i>g) Plan for, protect and preserve employment areas for current and future uses and ensure that the necessary infrastructure is provided to support current and projected needs;</i></p> <p><i>h) Protect employment areas in proximity to major good movement facilities and corridors for employment uses that require those locations and which can be serviced</i></p>	<p>The proposed development is for the division of land only. The Employment Area Official Plan designation and GM Zoning for the subject property is to remain, and approval of the consent will not affect the viability and range of employment uses to meet long-term needs.</p> <p>The subject lands will have access to municipal water and sewer services, which are now available, allowing the proposed development to make efficient use of municipal infrastructure.</p>















COUNTY OF NORTHUMBERLAND OFFICIAL PLAN POLICIES	PLANNING ANALYSIS
<i>by full municipal sewer and water services within the planning period;</i>	
<b>E GENERAL DEVELOPMENT POLICIES</b>	
<b>E1 SUBDIVISION OF LAND</b>	
<b>E1.4 SUBDIVISION REVIEW CONSIDERATIONS</b>	
<p><i>Prior to the consideration of an application for Plan of Subdivision, the approval authority shall be satisfied that:</i></p> <p><i>a) The approval of the development is not premature and is in the public Interest;</i></p>	<p>The proposed development represents the logical and compatible expansion as the site abuts a built-up, industrial area within the Cobourg urban settlement area. Development surrounds the site and municipal services are available. On this basis, the Kerr Street subdivision is not considered premature and its approval is in the public interest.</p>
<p><i>b) There is reserve sewage system capacity and reserve water system capacity available in the municipal water and sewage treatment systems, if such services are to be provided;</i></p>	<p>A Functional Servicing Study has been completed in support of the Kerr Street subdivision. It confirms there is sufficient reserve capacity available in the municipal sewage and water system.</p>
<p><i>e) Stormwater management plans shall be required for any new development consisting of more than four lots or for commercial or industrial developments with large amounts of impervious area. Stormwater management will be undertaken in accordance with MOECC Guideline “Stormwater Management Planning and Design Manual, 2003.</i></p> <p><i>The municipality shall require the use of stormwater management facilities downstream of new developments, where appropriate, to mitigate development</i></p>	<p>A Stormwater Management Report has been prepared in support of the Kerr Street subdivision. It details a plan to provide shared quantity control for the subdivision along with erosion and sedimentary control measures in accordance with MOECC guidelines. Quality control measures (i.e., side yard grassed swales and grassed filter strips) will need to be evaluated during site plan approval process. Quality control is to be provided and be subject to Site Plan Control on a per lot basis.</p>

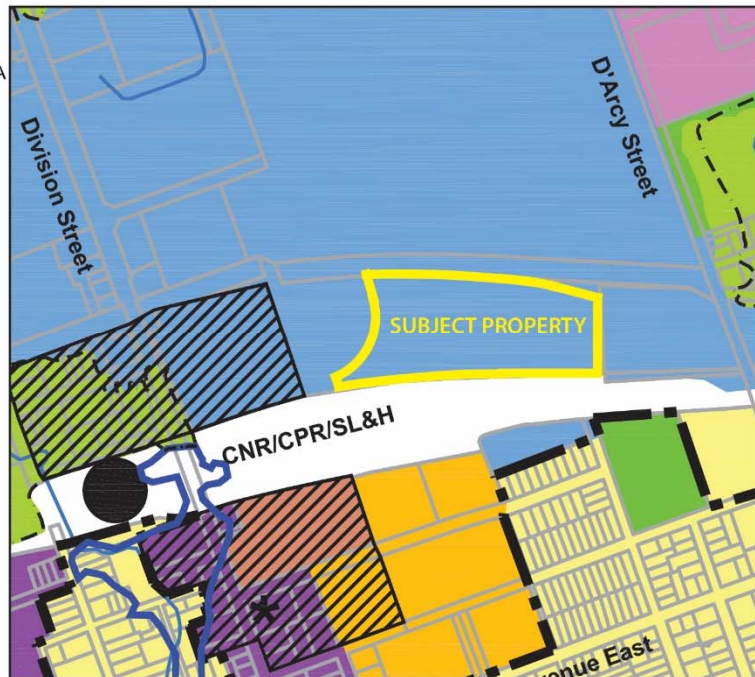
COUNTY OF NORTHUMBERLAND OFFICIAL PLAN POLICIES	PLANNING ANALYSIS
<p><i>impacts on stormwater quantity and quality. The municipality shall promote naturalized stormwater management facilities, constructed with gentle slopes. Applications for development shall be required to be supported by a stormwater quality/quantity management study.</i></p>	
<p><i>f) The lands will otherwise be appropriately serviced with infrastructure, schools, parkland and open space, community facilities and other amenities;</i></p>	<p>Existing infrastructure, such as the Cobourg train station (approximately 485 metres to the west) and the multi-use path along the newly constructed portion of Kerr Street between Division Street and D'Arcy Street will appropriately service the general industrial uses of the Kerr Street subdivision.</p>
<p><i>h) The subdivision, when developed, will be appropriately integrated with other development in the area; and,</i></p>	<p>The subdivision will integrate into the established urban fabric. The existing conditions (i.e., Kerr Street arterial, CNR main line, GM lands, and industrial development to the north) surrounding the Kerr Street subdivision are complementary uses and the proposed development is considered appropriate on this basis.</p>
<p><i>i) The proposal has regard to Section 51 (24) of the Planning Act, as amended.</i></p>	<p><b>See Table 6: Planning Act and Analysis</b> below for detail.</p>

## 6. CONFORMITY TO THE TOWN OF COBOURG OFFICIAL PLAN (2017)

The Town of Cobourg Official Plan was approved the Ministry of Municipal Affairs and Housing (MMAH) in May, 2017. The subject lands are designated “Employment Area” and within the designated “Built Boundary” on Schedule ‘A’ Land Use Plan of the Town’s Official Plan. Kerr Street is designated “Proposed Arterial” on Schedule ‘E’ Transportation Plan. Given the portion of Kerr Street newly constructed along the subject property, it is presumed this is now “Existing Arterial”. It is also important to note that the site abuts a “Major Transit Station Area”.

**LEGEND**

-  ENVIRONMENTAL CONSTRAINT AREA
-  EMPLOYMENT AREA
-  HIGH DENSITY RESIDENTIAL AREA
-  MAIN CENTRAL AREA
-  MAJOR INSTITUTIONAL
-  MIXED USE CORRIDOR AREA
-  PARK AREA
-  RESIDENTIAL AREA
-  \* SPECIAL PROVISION
-  STABLE RESIDENTIAL AREA
-  MAJOR TRANSIT STATION
-  MAJOR TRANSIT STATION AREA



**Figure 5: Schedule A: Land Use Plan Extract to Town of Cobourg Official Plan – Subject property outlined in yellow**

**Table 5** analyzes how the Kerr Street subdivision conforms to the Town of Cobourg Official Plan.

**Table 5: Town of Cobourg Official Plan Analysis**

TOWN OF COBOURG OFFICIAL PLAN POLICIES	PLANNING ANALYSIS
<b>2. COMMUNITY VISION, PRINCIPLES AND OBJECTIVES</b>	
<b>2.6 COMMERCIAL DEVELOPMENT PRINCIPLE: FINANCIAL FEASIBILITY</b>	
<p><i>The following objectives support this community development principle:</i></p> <ul style="list-style-type: none"> <li><i>i) To keep the capital and operating costs of public sector development in the Town of Cobourg at a level that can be borne financially by residents and businesses;</i></li> <li><i>iii) To enhance the community's economic viability through the provision of opportunities for employment uses and the promotion of such development;</i></li> </ul>	<p>The Kerr Street extension project is a Town-led initiative and is independent from development of the subject property. It is presumed though that the investment in Kerr Street is intended to act as a catalyst to spur private investment in an underutilized/vacant area of the Northam Industrial Park. The applicant has been contemplating subdividing the subject property for several years, and the recently completed portion of Kerr Street will indeed facilitate private development and investment of the site. As the subject lands are currently vacant, the proposed development will aid in bringing more economic vitality to the area, along with a larger tax base and development charges for the Town.</p>
<b>2.7 DESIGN PRINCIPLES</b>	
<p><i>iii) Promote Active Transportation – Active transportation will promote the priority of pedestrian, cyclists and transit over vehicles through the design of streets and their boulevards</i></p>	<p>The Kerr Street subdivision has frontage along a newly constructed section of Kerr Street consisting of a multi-use path that will facilitate active transportation.</p>
<p><i>vi) Provide a Vital Setting for Employment Uses – A healthy mix of employment uses, including innovative employment opportunities, and the placement of employment lands in key locations will ensure the Town of Cobourg retains its role as a vibrant employment centre in Northumberland County. High profile office, prestige employment and mixed use buildings will be located in the most visible sites to reinforce this image.</i></p>	<p>The Kerr Street subdivision is within a designated Employment Area and a General Industrial (GM) Zone, which permit a range of employment uses. This, along with the site being 485 metres from the Cobourg train station and consisting of a CNR spur line, contribute to a vital setting for employment uses.</p>

TOWN OF COBOURG OFFICIAL PLAN POLICIES	PLANNING ANALYSIS
<b>3. LAND USE STRATEGY</b>	
<b>3.2 COMMUNITY STRUCTURE AND GROWTH MANAGEMENT STRATEGY</b>	
<b>3.2.1 Existing Community Structure</b>	
<p><i>v) Employment Areas: Employment areas are primarily developed for industrial and related uses.</i></p>	<p>The current Employment Area Official Plan designation and GM Zone are to remain.</p>
<b>3.10 EMPLOYMENT AREA</b>	
<b>3.10.5 Land Use Policies</b>	
<b>3.10.5.1 Employment Area Zones</b>	
<p><i>Employment Areas shall be planned and developed to ensure a diversity of serviced sites across the Town for industrial/business uses. The zoning by-law shall establish a range of zones to permit the development of different employment areas, recognizing that certain uses, as well as other employment uses. The following zoning approaches shall be used where applicable:</i></p> <p><i>i) General Industrial Zone: This zone will permit the full range of industrial uses including operations which may be offensive because of heat, smoke, noise, odour, dust or heavy transportation requirements. Open storage shall be permitted in this zone subject to specific regulations.</i></p>	<p>The subject property is currently zoned General Industrial (GM). Approval of the Kerr Street subdivision will not require an amendment to the Zoning By-law.</p>
<b>3.10.5.2 Employment Area Design Considerations</b>	
<p><i>Employment Areas shall be planned and designed to ensure that:</i></p> <p><i>i) areas are of sufficient size to provide for a range of site sizes, adequate and appropriate infrastructure, and adequate buffering;</i></p>	<p>Lot sizes within the Kerr Street subdivision range from 5 times to 10 times the minimum GM Zone lot area and will contribute to a mix of employment site sizes. There are no specific building plans available at this time and required buffering to sensitive land uses is currently unknown. If buffering or other mitigation is required, Site Plan Control would be an</p>

TOWN OF COBOURG OFFICIAL PLAN POLICIES	PLANNING ANALYSIS
	appropriate method of implementing these measures.
<b>3.16 MAJOR TRANSIT STATION AND MAJOR TRANSIT STATION AREA</b>	
<p><i>iii) The Major Transit Station Area designation is an overlay designation. Within this area, the Town will encourage increased density of development to support the viability of train service. The Town may permit increased density of development above that permitted in the underlying land use designation in accordance with the policies of Section 9.2.6, Height and Density Bonus Provisions.</i></p>	<p>The subject property is not within the Major Transit Station Area overlay designation; however, it directly abuts this area. Due to this proximity, increased density of the site is anticipated to further assist with supporting the viability of train service, being 485 metres from the station, which is encouraged.</p>

## 7. PLANNING ACT, RSO 1990, SECTION 51 (24) ANALYSIS

The Planning Act establishes criteria in Section 51 (24) that the approval authority must have regard for when approving a draft plan of subdivision. Regard must be given, among other matters, to the health, safety, convenience, accessibility for persons with disabilities and welfare of the present and future inhabitants, and to the following:

**Table 6** demonstrates how the proposal addresses Section 51(24) of the Planning Act.

**Table 6: Planning Act and Analysis**

SECTION 51(24)	PLANNING ANALYSIS
<i>a) The effect of the proposed subdivision on matters of provincial interest, as referred to in Section 2 of the Act;</i>	The Application for Approval of a Plan of Subdivision is consistent with the 2020 PPS and will address the applicable Provincial interests set out in Section 2 (a-q) of the <u>Planning Act</u> .
<i>b) Whether the proposed subdivision is premature or in the public interest;</i>	The proposed development represents the logical and compatible expansion as the site abuts a built-up, industrial area within the Cobourg urban settlement area. Development surrounds the site and municipal services are available. On this basis, the Kerr Street subdivision is not considered premature.
<i>c) Whether the plan conforms to the Official Plan and adjacent plans of subdivision;</i>	The Draft Plan of Subdivision conforms to the Town of Cobourg Official Plan (see <b>Table 5</b> above). The subdivision will integrate into the established urban fabric. The existing conditions (i.e., Kerr Street arterial, CNR main line, GM lands, and industrial development to the north) surrounding the Kerr Street subdivision are complementary uses and the proposed development is considered appropriate on this basis.

SECTION 51(24)	PLANNING ANALYSIS
<p><i>d) The suitability of the land for the purposes for which it is to be subdivided;</i></p>	<p>The subject property is generally flat and gently slopes towards the south west. The property is currently vacant and fronts on a newly constructed Kerr street extension. This development will help the Town of Cobourg recuperate their investment in the extension. Sufficient area exists outside of appropriate railway setbacks to ensure the protection of human safety and property. There is also sufficient area outside of the required access easements and storm water easements to provide suitable development land.</p>
<p><i>e) The number, width, location and proposed grades and elevations of highways and the adequacy of them, and the highways linking the highways in the proposed subdivision with the established highway system in the vicinity and the adequacy of them;</i></p>	<p>Improvements to Kerr Street have not been identified through pre-consultation for the proposed development. The immediate portion of Kerr Street is newly constructed and there are no apparent concerns regarding its adequacy as an arterial to accommodate the Kerr Street subdivision.</p>
<p><i>f) The dimensions and shapes of the proposed lots;</i></p>	<p>The dimensions and shapes of the proposed lots are regular and appropriate for the area.</p>
<p><i>g) The restrictions or proposed restrictions on the lands to be subdivided or adjoining lands;</i></p>	<p>The lands are subject to the policies of the Town of Cobourg Official Plan and Zoning By-law. The proposed plan will be subject to comprehensive conditions of draft approval and a Subdivision Agreement.</p>
<p><i>h) Conservation of natural resources and flood control;</i></p>	<p>A Tree Inventory and Assessment was prepared by Cressman Tree Maintenance &amp; Landscaping Ltd., dated January 18 2021, has been prepared in support of the applications. It has been determined that the</p>



SECTION 51(24)	PLANNING ANALYSIS
	condition and species of trees on the site do not warrant preservation.
<i>i) The adequacy of utilities and municipal services;</i>	Utilities and municipal services are available and will be adequate to service the proposed density. Hydro is available and natural gas is planned to be extended to the proposed new lots as they develop.
<i>j) The adequacy of school sites;</i>	Not applicable.
<i>k) The area of land, excluding highways, to be dedicated for public purposes;</i>	No land is required or proposed to be dedicated for public purposes – access and storm water management will be private. Parkland will be provided as a cash-in-lieu payment.
<i>l) The extent to which the plan’s design optimizes the available supply, efficient use and conservation of energy;</i>	Urban development is a means of energy conservation as there is minimal loss of power from service delivery, which is inherently more energy efficient. The site is also accessible by multiple modes of transportation, vehicular, train (passenger and freight) and active, which may facilitate a reduction in vehicle trips whereby mitigating greenhouse gas emissions.
<i>m) The interrelationship between the design of the proposed plan of subdivision and site plan control matters relating to development on the lands, if the lands are located within a site plan control area designated under Section 41 of the <u>Planning Act</u>.</i>	The plan of subdivision will be controlled by draft conditions of approval and a Subdivision Agreement approved by the Town, as well as Zoning By-Law controls. Site Plan Control will be required for Lot 1 to 6.

## 8. ZONING BY-LAW NO. 85-2003 ANALYSIS

The subject property is currently within the “General (GM) Zone” on Schedule ‘A’ to By-law No. 85-2003 as amended. An Application for an Amendment to the Zoning By-law is not required to implement the Kerr Street subdivision.

**LEGEND - ZONING**

	R1 - RESIDENTIAL 1
	R2 - RESIDENTIAL 2
	R3 - RESIDENTIAL 3
	R4 - MULTIPLE RESIDENTIAL 4
	R5 - HIGH DENSITY RESIDENTIAL 5
	NC - NEIGHBOURHOOD COMMERCIAL
	SC - SHOPPING CENTRE COMMERCIAL
	DC - DISTRICT COMMERCIAL
	MC - MAIN CENTRAL COMMERCIAL
	LM - LIGHT INDUSTRIAL
	GM - GENERAL INDUSTRIAL
	BP - BUSINESS PARK
	I - INSTITUTIONAL
	OS - OPEN SPACE
	EC - ENVIRONMENTAL CONSTRAINT

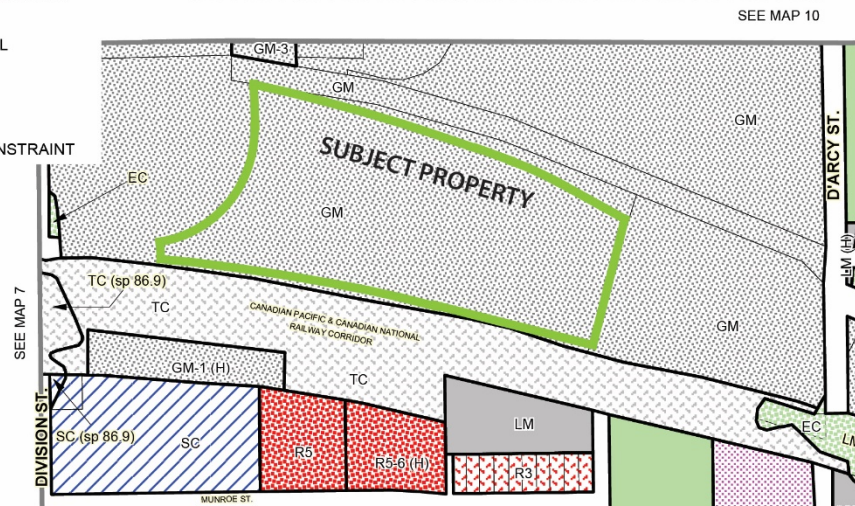


Figure 6: Schedule 6 Extract to Town of Cobourg Zoning Bylaw No. 85-2003

Figure 4 – Development Site Plan forms the basis of the zoning analysis below. The required and proposed zone provisions are shown in Table 6 below.

**Table 7: GM Zoning Analysis**

<b>Zoning Regulations – Section 17</b>	<b>GM Zone</b>	<b>Proposed</b>
Lot Area (minimum)	1,400 m <sup>2</sup>	7,088 m <sup>2</sup>
Lot Frontage (minimum)	30 m	52.6 m
Lot Coverage (maximum)	60 %	42 %
Front Yard (minimum)	9 m	19.9 m
Rear Yard (minimum)	7.5 m	30 m
Interior Side Yard (minimum)	3 m	4 m
Exterior Side Yard (minimum)	9 m	N/A
Landscaped Open Space (minimum)	10 %	19.1 %
Building Height (maximum)	13 m	13 m
Parking and Loading (minimum per 100m <sup>2</sup> GFA)	1.0	1.1

To reiterate, the Kerr Street subdivision project does not require any rezoning in order to implement the proposed lot configuration or uses. The plan complies with all General Industrial (GM) Zone provisions.

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## 9. PLANNING OPINION AND CONCLUSION

The subject property is located within the Employment Area designation of the Town of Cobourg Official Plan and is within the Urban Settlement Area Boundary. It is also designated as a Delineated Built-up Area within the Growth Plan and designated Urban Area within the County of Northumberland Official Plan. The subject property is currently vacant.

The subject property has frontage on the south side of Kerr Street. It is proposed to create six (6) lots for general industrial use to be serviced by municipal water and sanitary systems, which are now available. This will optimize use of existing infrastructure and increase the economic vitality of the area. The Cobourg urban area shall be the focus of growth, and intensification is encouraged. The Kerr Street subdivision abuts a Canadian National Railway spur line and is freight supportive, along with fronting onto Kerr Street, an arterial, which has recently completed construction and includes a multi-use path. These characteristics will facilitate active transportation and continue to be freight supportive.

Design and subdivision policies of the Cobourg and County Official Plans that set out criteria to be considered when developing within this area have guided the planning process for the project. The proposed subdivision is surrounded by an existing built-up area, will contribute toward infill/vacant-lot intensification, is supported by available infrastructure and consists of no apparent traffic hazards. On this basis, it is our professional planning opinion that there are sufficient controls through the Site Plan Control process to ensure that site-specific development plans will be compatible with surrounding land uses and maintain a positive image for the Town of Cobourg. The proposal maintains the general intent and purpose of the Official Plan.

It is our professional planning opinion that the Application for Approval of a Plan of Subdivision for the subject property is consistent with the policies of the Provincial Policy Statement and Growth Plan and conform to the County and Cobourg Official Plans. The proposed development will comply with all General Industrial (GM) Zone provisions and general provisions of Zoning By-law No. 85-2003, and represents good planning.

If you have any questions about this information, please do not hesitate to contact our office at 613-966-9070.

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## 10. REPORT SIGNATURE

Yours truly,



Shawn Legere, MCIP, RPP  
Senior Planner  
RFA Planning Consultant Inc.



Alex Cleave, B.Comm  
Planner  
RFA Planning Consultant Inc.

