



**R. W. BRUYNSON INC.**  
ARCHITECT • CONSULTING ENGINEER

## URBAN DESIGN & SUSTAINABILITY REPORT

Joshani Homes  
377 William Street  
Town of Cobourg  
March 6, 2020  
Our File 18077

Prepared by R.W. Bruynson, P.Eng., OAA

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## 1.0 Introduction

### 1.1 Introduction

On November 15, 2019, R.W. BRUYN SON INC. submitted an application for a review of a Plan of Subdivision to the Town of Cobourg Planning Department for 377 William Street. The intention was to develop an infill parcel of land into freehold townhouses and semi-detached residences with a common element entity in the form of a privately owned and operated lane. The land was the location of a former automobile service station which had subsequently been the subject of a higher density condominium townhouse project. Ultimately, the land usage was revised by the present owners to lessen the density and change the development into the present form. On February 20, 2019, a meeting of the Committee of Adjustments was held at the Town hall to request relief on some minor issues with the present proposal from what was approved in the By-Law or a previous submission. Having no public or staff objections to the request, the development was furthered with additional information being requested by the governing authorities.

### 1.2 Pre-consultation

A pre-consultation meeting on March 13, 2019 was held at the Town hall to discuss any major issues with the proposal. In attendance were persons from various departments and governing authorities voicing issues of concern and requesting additional information and reports. A letter outlining these matters was previously sent to the client on May 1, 2018.





### 1.3 Studies and Reports

Our recent submission included the following reports and studies:

- 1) Site Plan
- 2) Architectural Elevations and Perspective Drawings
- 3) Servicing and Grading Plans
- 4) Stormwater Management Report and Plans
- 5) Landscape Plan
- 6) Lighting Illumination Plan
- 7) Arborist's Report
- 8) Surveyor's R-Plan
- 9) Noise Attenuation Report
- 10) Geotechnical Report
- 11) Environmental Report
- 12) Completed application for Plan of Subdivision
- 13) Authorization of Agent by Owner

Not included but requested in the May 1, 2018 letter, re-iterated in correspondence dated November 28, 2019 and subsequently submitted were the following

- 14) North-South and East-West cross sections through the development.
- 15) Traffic Impact Assessment
- 16) Urban Design & Sustainability Brief
- 17) Topographical Survey

## 2.0 Town of Cobourg Official Plan

The Town of Cobourg Official Plan was reviewed for relevant sections and itemized below.

### The Official Plan sustainability strategy policy for development form

- i) *reduce the consumption of energy, land and other non-renewable resources including support for energy efficient buildings and opportunities for co-generation;*

The proposed development is within the established Town boundaries and is considered an in-fill property. The facilities serving this development are presently existing and adequate. The proposed buildings consist of energy efficient townhouses and semi-detached which, being that they are surrounded by single detached dwellings to the south and west and a proposed multi storey residential complex to the north provide a gradual transition from high density to low density.

- ii) *minimize the waste of materials, water and other limited resources;*  
This issue will be best addressed in the final design and construction documents

- iii) *create livable, healthy and productive environments; and,*



Providing a spacious living environment with improved and enhanced landscaping conducive to the surrounding properties will offer an inviting place for families to settle and enjoy their life style in affordable dwellings.

iv) *reduce greenhouse gases.*

Being close to downtown and within easy walking distance coupled with only a short distance to the shopping centers in the north-west sector of the Town will reduce the need for long vehicular trips.

v) *enhance biodiversity, ecological function, and the natural heritage system, including the provision of wildlife habitat and linkages.*

This development does not have any natural land or water features except random trees. As many of these trees, as economically and functionally possible, will be saved. Enhanced landscaping is proposed to supplement and expand on those that are felled in order to provide an environment that is not only orderly and coordinated, but will thrive and mature over the decades to come.

**The Official Plan street system general design policies:**

i) *Street patterns in newly developing areas will be designed to reflect an interconnected street system with a modified grid pattern or other approaches, which facilitate continuous and direct movement within a development area, and between the development area, abutting areas and the existing community. In addition, blocks should be limited in length to reduce the need for mid-block connections and promote active transportation.*

The development has only one frontage onto William Street. Albeit, it has easement rights from the rear onto Margaret Street, this easement is limited in width and is restricted to underground services only which preclude its use for vehicular or pedestrian traffic. Due to the width of the property, this development proposes a single private lane to service all the dwellings. The lane will be deemed a common element and maintained by all the owners of the dwellings it serves. The connection to William Street and its impact is addressed in the traffic report prepared by Candevcon Consultants.

ii) *Street patterns in existing areas shall not be modified except:*

- a. *Where the rights-of-ways are significantly below Town standards and the need for widening has been identified in the 1992 Transportation Study or an update of that study, recognizing that in Heritage Conservation Districts generally widening shall not be permitted;*

No modifications are proposed

b. *To introduce traffic calming techniques*

The development proposes only 14 residential units entering and existing onto and off William Street. This egress is close to the University Avenue intersection but not beyond the proximity of the curb median. This issue is further addressed in the Candevcon report.

- c. Through the introduction of boulevards, wide sidewalks, landscaped medians or other techniques on arterial and collector roads to mitigate the visual impact of existing wide rights-of-way;*

The frontage of the property is along William Street where there is a re-constructed sidewalk proposed. This sidewalk is designed to be 1.5 metres wide and extend across the entrance.

- d. The introduction of bicycle paths and lanes, as appropriate to the function of the road;*

The development is serviced by only one lane with only one access point. As such, the vehicular traffic will be limited within this development providing decreased vehicular conflict. The development also proposed a 1.5 metre sidewalk along the south side of the lane connecting to the sidewalk along William Street.

- e. The introduction of additional streets to improve connectivity within the street system; and*

No other streets are possible.

- f. Other modifications deemed necessary by the Town to improve the operation of the street.*

Additional requests from the Town will be addressed upon receiving comments from this submission.

### **The Official Plan streetscape design policies**

- i) there is not reverse lotting*

There is no reverse lotting proposed

- ii) there is unobstructed street frontage adjacent to public open space and Environmental Constraint Areas, where appropriate, and subject to appropriate design to ensure that there will be no negative environmental impacts;*

There are no public open spaces or Environmental Constraint Areas

- iii) pavement, sidewalk and right-of-way widths, as well as the provision of bicycle lanes and paths, reflect the function of the street;*

There is a 6 metre private lane with an adjacent 1.5 metre sidewalk to safely accommodate all vehicular and pedestrian traffic. These components are linked to the system on William Street.

- iv) the siting of parking areas, and the siting and massing of buildings and structures on all streets, but particularly on arterial and collector streets, shall generally assist in the creation of high quality streetscapes and, in particular, regard shall be had to providing a sense of enclosure to the street*



*by situating building mass adjacent to the street, and particularly at the intersections, to frame the street space;*

The configuration of the property and the requirements for access allows only a singly loaded siting. This creates adequate front yards and generous rear yard spaces. The bending of the lane to accommodate the semi-detached buildings at the rear of the property provides a non-linear flavour to the streetscape as well as communal visitor parking.

- v) *in residential areas, garages shall be designed so that they are not the dominant feature in the streetscape. In particular, garages shall not:*
  - a) *project beyond the façade of the residence; and,*
  - b) *dominate the frontage of the lot, unless plans are submitted by the applicant to demonstrate to the satisfaction of the Town that the garages can be appropriately integrated with the streetscape.*

All garages for both the semi-detached and townhouses are set in beyond the front face of the dwellings. The projecting living spaces in the front yards are either a den, dining room or an optional bedroom.

- vi) *landscaping and/or architectural features provides for features such as the definition of the street and public open space, framing of views and focal points, direction of pedestrian movement and demarcation of areas with different functions as appropriate through features such as low masonry walls, metal fences, rockery, special building and roofline treatments, landmark features (e.g. clocks, towers, cupolas, bays, pergolas, weather vanes, art), outdoor activity areas (e.g. patios, plazas, squares), extensive plant material and other identifiable features; and,*

Extensive landscaping is proposed not only along William Street, but along the front and rear yards of the individual lots. New trees are proposed strategically placed to enhance the lot esthetics yet avoid conflicts with the required services. Consideration has been given to shield the adjoining neighbours from the onsite activities by means of directional street lighting and hard and soft property line screening.

- vii) *service and open storage areas and utilities and services should be sited away from prominent views from the street to minimize impacts, alternative design options for locating such facilities should be considered, and only where there is no alternative should they be sited in view sensitive locations, in which case decorative screening shall be required.*

There is not open storage areas proposed. The electrical transformers are located away from the lot frontages as are the street lighting standards thus freeing up the front yards for landscaping purposes.

### ***The Official Plan view policies***

- i) *New development shall be designed to preserve, enhance and/or create significant views of the following features:*



- a) *Lake Ontario*
- b) *view from the harbour to the Victoria Hall clock tower in accordance with the requirements of the Harbour Area Secondary Plan;*
- c) *important institutional or other buildings;*
- d) *open spaces; and,*
- e) *natural features.*

Due to the location of the property, there is not a view possible of Lake Ontario, Victoria Hall or other features, significant buildings or open spaces. The storm water facility that often serves as a landscape feature is underground and provides no such opportunity.

#### **The Official Plan landscape policies**

*The Town shall ensure that appropriate landscaping is provided in new development and re-development to:*

- i) *maintain and enhance the character of existing developed areas;*

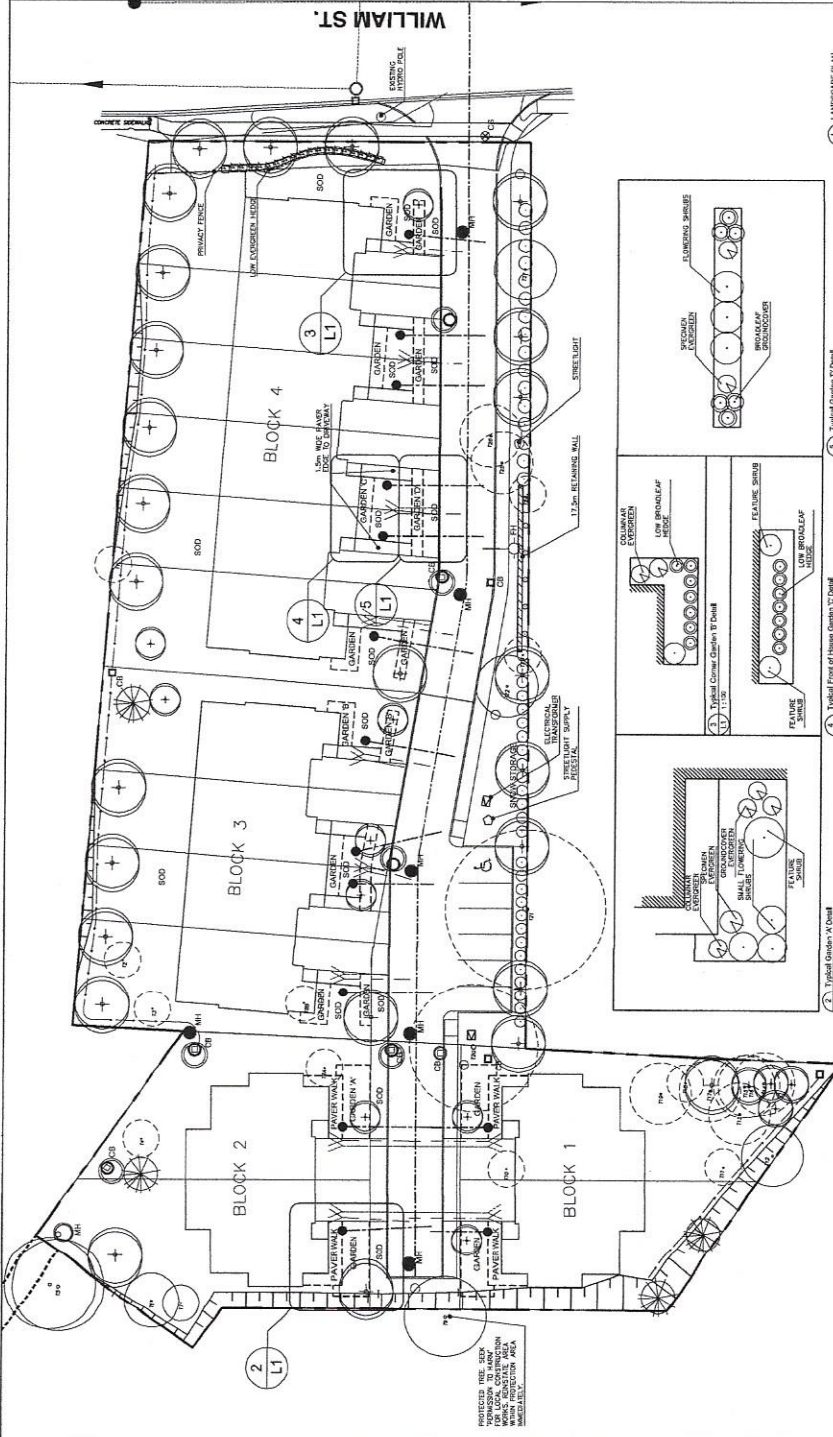
The site is presently vacant with mostly perimeter trees and only a couple located within the interior of the lot. Most of the trees will have to be removed to facilitate the development (buildings and services). However, it is planned to replace them with more robust varieties that will prove to be more complementary to the land usage and provide a sustainable environment.

- ii) *allow for the creation of strong landscaped features in newly developing areas, including storm water management facilities which shall be designed as important components of the Greenlands system, such that they will be incorporated in development in a manner which enhances views and access through street frontage; and,*

An extensive landscape plan has been designed to enhance the environmental esthetics. The storm sewer system is to be underground thus providing no opportunity for expanding the Greenlands system.

- iii) *protect and enhance environmental features and open space areas having regard for the policies of Section 4, Greenlands System.*

Due to the size and configuration of the site, there are limited features to be protected or linked with the Greenlands System.



**Base Plan Information By Others**  
 Refer to respective drawings for content  
 Site Plan (L1) - By: [Name]  
 Survey (Ind. Locates of Trees) - By: [Name]  
 Plan Date: June 14, 2019

**Legend**

- Tree to be preserved
- Tree to be removed
- New deciduous tree to be planted
- New evergreen tree to be planted
- Coniferous shrub
- Deciduous shrub
- Broadleaf shrub

**PLANT LIST**

Quantity	Symbol	Common Name	Notes
1	[Symbol]	Red Maple	Plant in Block 1
2	[Symbol]	White Birch	Plant in Block 2
3	[Symbol]	Black Birch	Plant in Block 3
4	[Symbol]	White Birch	Plant in Block 4
5	[Symbol]	Black Birch	Plant in Block 1
6	[Symbol]	White Birch	Plant in Block 2
7	[Symbol]	Black Birch	Plant in Block 3
8	[Symbol]	White Birch	Plant in Block 4
9	[Symbol]	Black Birch	Plant in Block 1
10	[Symbol]	White Birch	Plant in Block 2
11	[Symbol]	Black Birch	Plant in Block 3
12	[Symbol]	White Birch	Plant in Block 4
13	[Symbol]	Black Birch	Plant in Block 1
14	[Symbol]	White Birch	Plant in Block 2
15	[Symbol]	Black Birch	Plant in Block 3
16	[Symbol]	White Birch	Plant in Block 4
17	[Symbol]	Black Birch	Plant in Block 1
18	[Symbol]	White Birch	Plant in Block 2
19	[Symbol]	Black Birch	Plant in Block 3
20	[Symbol]	White Birch	Plant in Block 4

**GENERAL LANDSCAPING NOTES**

1. All plantings shall be done in accordance with the City of Toronto's Planting Standards and Specifications.

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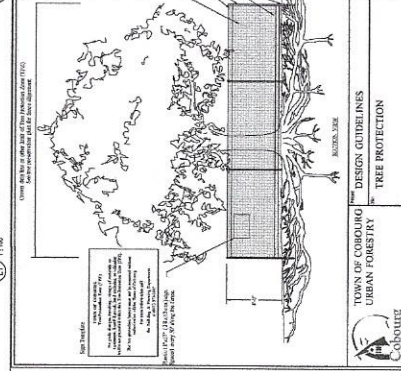
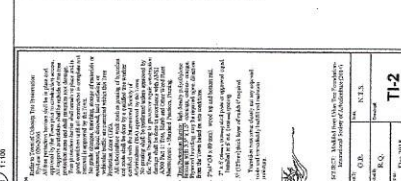
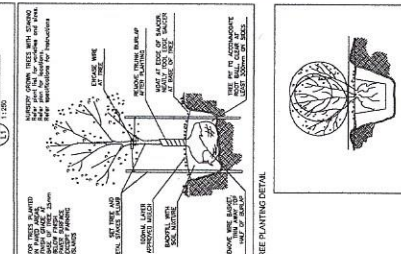
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10. All plantings shall be done in accordance with the City of Toronto's Planting Standards and Specifications.



**PROTECTION OF EXISTING TREES** - See also order report by Dreamworks Tree Services, Uxbridge Ontario, April 22, 2019, updated October 2019.

1. All trees to be preserved shall be marked with a red 'X' and a red circle.

2. All trees to be removed shall be marked with a red 'X' and a red circle.

3. All trees to be preserved shall be marked with a red 'X' and a red circle.

4. All trees to be removed shall be marked with a red 'X' and a red circle.

5. All trees to be preserved shall be marked with a red 'X' and a red circle.

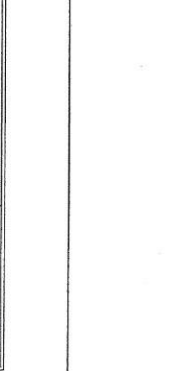
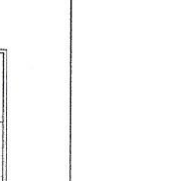
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### **The Official Plan safe community design policies**

*To promote safety and security in public places and minimize the potential for crime, urban design should result in clear, unobstructed views of areas such as parking lots, parks, public institutions and open spaces, while streetscapes should be designed to encourage active public use and natural surveillance opportunities. In particular, the following measures shall be followed:*

- i) the design and siting of new buildings and structures shall provide opportunities for visual overlook and ease of public access to adjacent streets, parks and open spaces;*

All the dwellings have living spaces with direct and unobstructed views of the front and rear yards as well as the visitor parking areas.

- ii) clear, unobstructed views to parks and open spaces shall be provided from adjoining streets;*

There are no open spaces or parks proposed in this plan.

- iii) appropriate lighting, visibility and opportunities for informal surveillance shall be provided for all walkways, parking lots, parking garages and open space areas;*

The sidewalk adjacent to the lane is fully illuminated with street standards and clearly visible along its entire length.

- iv) landscape elements shall be selected and sited in order to maintain views for safety and surveillance.*

The landscape features within the front yards are low shrubbery. There is no fencing proposed that would restrict the visibility within the property boundaries.

### **The Official Plan accessibility and visitability policies:**

*It shall be a policy of the Town to work towards the goal of equal access for all persons to public buildings and facilities, and to encourage the public and private sector to adopt similar goals with respect to their facilities as required to implement the Ontarians with Disabilities Act. The Town shall work with the Cobourg Accessibility Committee and citizens to continually refine and update a comprehensive, long term strategy for implementation which identifies barriers and proposes options to overcome them, allocates resources for education and change and monitors progress towards those ends. In particular, the principles of universal design should be applied to all public spaces and within new developments to ensure access and visitability for all individuals, while recognizing the need for balance where designated heritage properties or properties in Heritage Conservation Districts are involved. Regard shall also be given to the Ontario Building Code and the Ontarians with Disabilities Act, as well as the Town's Urban and Landscape Design Guidelines, the Heritage District Guidelines and, where appropriate, the Parks Canada Standards and Guidelines for the Conservation of Historic Places in Canada.*



The sidewalks and the appropriate number of visitors parking stalls have been accounted for in the development design. The design of the dwelling units will comply with the requirements of the Ontario Building Code.

### ***The Official Plan active transportation policies***

*In considering the design of public and private facilities, a key consideration shall be features, which contribute to enhancements to the ability for movement by pedestrians and bicyclists and other active transportation modes, as well as individuals with mobility challenges while recognizing the need for balanced in Heritage Conservation Districts.*

*These include additions to the Town's trail system, the use of wide sidewalks, and the provision of bicycle parking and other facilities for bicycles. In particular, in considering pedestrian movement on specific sites, the following shall apply:*

- i) direct and clearly defined pedestrian connections which form an extension of the public transportation system shall be required throughout sites with one main pedestrian connection between all buildings and all adjacent municipal streets and sidewalks, and secondary connections for improved pedestrian access as appropriate;*

A 1.5 metre sidewalk is proposed on the south side of the lane to accommodate the pedestrian traffic and is linked to the system in William Street.

- ii) principal pedestrian routes shall be functionally separated from parking and driveway areas and should utilize a variety of surface materials, textures, colour, landscaping and changes in elevation;*

The pedestrian routes are adjacent to the lane which has limited vehicular traffic, the speed of which should be significantly low given the length of the lane.

- iii) on large sites street furniture such as benches, waste receptacles, bicycle racks and pedestrian level lighting should be provided along main pedestrian routes where appropriate; and,*

Being a small site, there is not intension of providing any of such exterior amenities except for the lighting.

- iv) opportunities for pedestrian connections with adjacent sites should also be explored, and will be required wherever feasible.*

The sidewalk is linked to the system in William Street.

### **Official Plan external building design policies**

*When considering, as part of site plan approval, the external design of buildings and the design of the adjacent streetscape, the Town shall take into account all the policies of this Plan, particularly the policies of this Community Design Strategy. In addition, in*

*evaluating the external design of buildings the Town shall seek design which reflects the directions in the Town's Urban Design and Landscape Guidelines and, in particular;*

- i) includes sustainable building practices such as the use of green roofs and solar panels with the achievement of LEED, Energy Star, other similar certification or equivalent being encouraged;*

The buildings will be designed in conformance with the Ontario Building Code and with good engineering practice and the minimum performance standards will be met or exceeded. This will all be dealt with during the final design plan preparation.

- ii) complements the massing patterns, rhythms, character and context of adjacent existing development, while recognizing that built form evolves over time and that new buildings should not necessarily replicate existing buildings;*

The flavour of the design follows the existing surrounding dwellings with pitched roofs and brick and stone veneered exterior. Siding will only be used at the rear and second floor exterior walls where it is less visible or susceptible to damage and where it would be more conducive to the structure of the building.

- iii) clearly defines public and private spaces;*

There are no public areas in this proposal. There is only common element area which is clearly defined by the laneway.

- iv) is designed to incorporate the principles of universal design to ensure access and visitability for all individuals, while recognizing the need for balance where designated heritage properties and properties in Heritage Conservation Districts are involved;*

The design and grading have been coordinated to accommodate barrier free movement. Ramps to the side walks and close-to-grade first floors have been shown in the preliminary plans. Further modifications will take place during final design.

- v) is in scale with surrounding development;*

With the buildings being designed as 1 ½ storey in height or bungalofs, and having a land density between the single-family homes to the south and west and what is proposed to the north, the development will provide a smooth transition.

- vi) is designed to ensure that all buildings, and in particular, any commercial, mixed use or institutional buildings are designed to animate and create a positive interface with the street through:*

- a) the use of architecture and façade treatment (e.g. landmark materials) to reflect the heritage of the community;*

The design of the front façade of the dwellings will follow a traditional theme in harmony with the surrounding existing properties.

- b) front doors and generous real windows, or in some cases three-dimensional display windows, on any major walls facing streets;*

The front doors have numerous and large real windows from habitable rooms.



- c) *strong pedestrian connections to the street, while drive through uses, where permitted, are carefully positioned to minimize impacts on the street;*

A wide sidewalk serves to connect all occupants of the buildings to the street.

- d) *the location of outdoor activity areas (e.g. patios), landscaping and other site design elements.*

There are no outdoor amenities planned for the front yards. All outdoor activities will be carried out in the privately owned and designated rear yard areas.

### **The Official Plan signage policies**

*Provisions for signs within private development shall comply with the Town's Sign By-Law. In addition, the appearance of signs should reinforce the character of development through design and choice of colour, material and their placement at the entrance areas and on building facades. Signs should be integrated into the site plan and building design to ensure coordination of design.*

No signage has been proposed for this development. However, a common name and address identification will be specified and in accordance with the Town's Sign By-Law.

## **3.0 TOWN OF COBOURG URBAN AND LANDSCAPE DESIGN GUIDELINES**

Completed in 2010, the Town of Cobourg Urban and Landscape Design Guidelines are to be considered in the process of the all development designs being public or private. A review of these guidelines has been undertaken and addressed as follows;

### **The Guidelines for streets and streetscapes policies for Arterial Roads**

- i) *Arterial Roads are high capacity transportation roads that serve as major gateways into the Town of Cobourg. Examples include Elgin Street, King Street, Ontario Street, Burnham Street and Division Street. While these streets serve an important transportation role in the community, as the Town evolves, Arterial Roads should transition to a more urban character and include a high level of design in the pedestrian realm. This includes buildings with densities supportive of transit and alternative modes of transportation (such as cycling), and the provision of well landscaped pedestrian-oriented boulevards.*



The proposed development fronts onto William Street which is considered an Arterial Road. With this, the increased density will be well serviced by the public transit along this route. A linking sidewalk will also provide the desired pathway for pedestrian and cycling traffic.

- ii) *To ensure a functional/urban design balance between the boulevard and street pavement, Arterial Roads should be designed to include wide sidewalks, street trees, consistent paving, lighting and public art where appropriate.*

This frontage has a varying boulevard width due to its proximity to University Avenue. However, the re-constructed sidewalk along this frontage is designed to be 1.5 metres wide and is connected to the private sidewalk of the same width adjacent to the lane. Low shrubbery has been designated along this frontage as well to soften the visual appearance of the development.

### **The Guidelines for streets and streetscapes policies for Local Roads**

- i) *Similar to the earliest areas of the Town where the local road pattern is compact and well-connected, new local streets should promote a similar pattern for efficient walking, cycling and transit access. More recently, existing local street patterns, for example north of the railway, are wider, less continuous and result in residential enclaves of looped roads and cul-de-sacs and are therefore less conducive to well connected neighbourhoods. Local Roads should also allow for mature tree growth in a planted boulevard adjacent to the road or sidewalk within the public right-of-way.*

There is no local road owned or operated by the municipality. However, a 6 metre wide private lane is proposed which will be deemed “common element” for the townhouse and semi-detached dwellings. This lane is connected to the adjacent neighbourhoods at only William Street and will be landscaped along its entire length with trees and shrubbery.

### **The Guidelines for streets and streetscapes policies for sidewalks in mixed use and commercial areas:**

- i) *The boulevard (between curb edge and building face) in the mixed Use/Corridor areas and Commercial Areas should be a minimum width of 4.0 metres and be comprised of a 1.5 metre wide walkway and 2.5 metre wide boulevard that is constructed of a hard paved surface and/or landscaping.*

A 1.5 metre sidewalk is proposed to be re-constructed along William Street to join the existing adjacent sidewalks to the north and south. However, due to the proximity to the intersection of University Avenue and its multi-lane configuration, the boulevard width varies since the street lines remain parallel.

- ii) *At corners, consideration should be given to the widening of boulevards to provide enhanced sidewalk conditions that include decorative planting areas,*

*seating areas, increased sight lines, universal design marking and other amenities (i.e. fountain, public art).*

Low shrubbery has been proposed along William Street to provide decorative planting. The existing planting at the south side will be removed in order to increase visibility as requested by the neighbour and will be substituted with more favourable landscape features.

- iii) *Sidewalks should be coordinated with the design of feature paving across boulevards, intersections, crosswalks and driveways to ensure visibility and accessibility of the pedestrian network.*

The sidewalk has been taken across the entrance.

- iv) *Sidewalks should connect with adjoining recreational trail networks, wherever possible.*

There are no adjoining trails.

- v) *All sidewalks shall be barrier free. Sidewalk clutter (e.g. newspaper boxes, signage, etc.), should be minimized to enable safe and efficient movement of pedestrians (including strollers, wheel chairs, etc.)*

The sidewalk is designed to be barrier free.

- vi) *As provincially mandated, curb ramps should be used to provide assistance to persons with disabilities, as well as providing a proper transition between the road surface and top-of-curb at pedestrian sidewalk corners.*

The sidewalk has been designed to provide curb ramps where it crosses the entrance to the development.

#### **The Guidelines for streets and streetscapes policies for sidewalks in residential areas:**

- i) *Residential area sidewalks should be a minimum of 1.5 meters wide and be provided on both sides of all residential streets.*

There is a proposed 1.5 metre sidewalk along the south side of the lane. This location allows more useable area for the lots in both front and rear yards.

- ii) *The design of sidewalks should be coordinated with intersecting driveways and private pedestrian walkways.*

There is no conflict with any intersecting driveways.

- iii) *All sidewalks shall be barrier free.*

The sidewalk has been modified to provide curb ramps at both ends and intermittently to facilitate easy access. The lane will have limited traffic which should not interfere with pedestrian access to the sidewalk.



- iv) *Generally, the sidewalk surface should be constructed of poured concrete, however, unit paving may be used as an edge condition on the sidewalk to provide opportunities for variation and visual interest.*

The sidewalk is to be constructed of poured concrete.

- v) *Street trees are generally recommended to be planted back from the sidewalk (i.e. away from the roadway) to prevent damage from salts and confined soil area to promote mature growth. However, street trees may be planted within a landscaped boulevard (minimum 2.5 metres wide) beside the curb edge.*

All trees are located away from the lane and on development property.

#### **The Guidelines for streets and streetscapes policies for cross walks and intersections:**

- i) *Crosswalks should be continuous and connected to adjacent sidewalks.*

A new re-constructed sidewalk is proposed along the frontage of William Street joining the existing on both sides. It is intended to continue the sidewalk through the entrance.

- ii) *Universal access should be provided at all crosswalks, including special surface treatments to facilitate access for the visually impaired.*

Curb ramps are designated where the sidewalk approaches the entrance.

- iii) *Crosswalks should be clearly designated for safety, with appropriate surface marking or variation on construction material and signs.*

This issue to be addressed during the detailed design.

- iv) *Areas with high pedestrian traffic, such as the Mixed Use/Corridor Area, should use feature paving or other marking, and should have pedestrian priority signalization, to reinforce pedestrian priority.*

The sidewalk crossing design has been modified to exhibit brick paving thus delineating pedestrian priority.

- v) *Streetscape design should take into account the geometry of streets and their sightlines. Transit shelters, signs, trees and other visual obstructions should be located to ensure they do not obstruct driver visibility and create unsafe conditions at intersections.*

Measures have been designed to reduce visual obstructions at lane/street intersection by means of removing existing bushes on the south and proposing low shrubbery and fences.

#### **The Guidelines relating to universal design policies for the private realm:**

- *The principles of universal design should be applied in all private realm developments to insure access for individuals of varying ability.*
- *For more detailed guidelines and standards, please refer to:*



- 1) *Section 3.4.5 Universal Design (Public Realm)*
- 2) *Ontario Building Code (Section 3.8; Barrier Free Design)*
- 3) *Ontarian With Disabilities Act (AODA)*
- 4) *Northumberland County Design Standards for Accessibility*
- 5) *Principles of Universal Design*

- i) *The design of buildings other than single, semi-detached or townhouses should result in accessibility for everyone.*

Notwithstanding the above exception, the dwellings are graded to provide ample barrier free opportunities. The detailed design will come at a later stage.

- ii) *At a minimum, design choices relating to circulation and building access for pedestrians and vehicles should conform to barrier-free access requirements as set out in Section 3.8 of the Ontario Building Code (OBC)*

The building design will conform to the Ontario Building Code 3.8. There is also barrier free consideration given to the visitors parking.

- iii) *Barrier-free access to the ground level of all publicly accessible buildings should be provided. Access structures such as ramps should be designed to harmonize with buildings.*

There are no public buildings proposed.

- iv) *Curb ramps should provide barrier-free connections between the street and pedestrian walkways.*

Numerous curb ramps have been proposed and later modified on plan.

- v) *All public sidewalks shall be barrier-free. Street trees and landscaping, seating, public art and signage should not be an obstacle to the barrier-free path of travel.*

All sidewalks are barrier free and have no obstructions.

- vi) *In high activity areas such as the Mixed Use/Corridor Area, Downtown, the Harbour and public parks, the use of multi-sensory visual and audio queues as well as textured paving should be considered to assist in orientation and the existence of potential hazards to disabled individuals. Sensory indicators may be tactile or audible.*

The plans have been modified to indicate a brick pavement at the crosswalk of the lane entrance.

#### **The Guidelines relating to on-street parking policies:**

- i) *Parallel on-street parking is preferred over perpendicular or angled parking to minimize the overall width of the roadway and optimize sightlines.*

Due to the configuration of the property and the jog in the laneway, perpendicular parking was thought to be more appropriate and easier to accommodate.

ii) *On-street parking may be situated within bump-outs where appropriate.*  
Other than the jog, no bump-outs are planned.

iii) *Bump-outs should be landscaped with street trees or low-level ground cover and be designed to accommodate snow loading.*

The bump-out has been designed with landscaping. It also serves as the location for a transformer and a light standard.

iv) *Where appropriate, permeable paving should be considered to promote drainage and enhance the street edge.*

Underground storm retention has been designed by Candevcon. There has been no consideration given to ground water recharge other than in areas of soft or grassed landscaping.

#### **The Guidelines relating to building typology policies for residential buildings:**

i) **Create a Strong Public Face:** *As the Town of Cobourg evolves, a large amount of residential development will remain as low-rise single and semi-detached buildings on local streets. The houses that line these streets substantially influence the image and pedestrian experience of the streetscape. House designs that accentuate an attractive and animated building frontage using elements including large windows, front porches and steps combined with architectural variety, will contribute positively to the streetscape and aid in casual surveillance opportunities. Garages should not be the dominate feature of the house and should not preclude opportunities to have useable rooms that look out onto the street.*

The development is designed to for strictly low rise semi-detached and townhouse dwellings as 1 ½ storey structures or bungalofs. The front façade consists of covered porches, brick and stone veneer, brick soldier coursing over the windows, dormers, etc. enhancing the lane view of the dwellings.

ii) **Automobile Storage Should Be Subordinate:** *To reduce the impact of automobile storage, the house façade should have greater expression than the garage through a well articulated façade.*

All garages are set back from the front of the building envelope and their impact reduced by proportionally large windows and front projecting living areas.

iii) **Create Dual frontages On Corner Lots:** *On corner lots, give positive expression to the two street frontages through the use of wrap-around front porches or sunrooms, bay windows and side entrances, where possible. Privacy fencing should be limited to screening the back your only.*

Corner and side lots have enhances features in windows and planar relief as well as a mixture of exterior building materials.



- iv) **Ensure Creative, High-Quality and Diverse Design:** *Housing design is intended to encourage creativity and diverse interpretation of architecture. The design guidelines will enable a variety of housing projects and styles while still creating cohesive, integrated and attractive neighbourhoods.*

The design projects the traditional concept of architectural creativity yet is harmonious in its transition from one unit to the next. The end townhouse units having a slightly different layout due to their side exposure still keeps within the uniformity of the block. The semi-detached have a side loaded entrance as a departure from the typical front entry.

- v) **Activity & Safety:** *An animated residential streetscape is a key design consideration. Housing should incorporate designs with habitable, street facing rooms (i.e. living, dining rooms and kitchens) to promote neighborhood safety through "eyes on the street".*

All the dwellings have habitable rooms facing the lane be it a bedroom, den or dining room.

- vi) **Context Sensitive:** *The mass, scale and architectural elements of residential buildings should be sensitive to adjoining areas.*

The height of the buildings will be no more than a typical 2 storey residential building. This is keeping in tune with the existing surrounding residences.

- vii) **Housing Variety & Choice:** *A full range of housing types (i.e. detached, semi-detached, townhouse, apartments, etc.) should be provided to accommodate a wide demographic (i.e. couples, families with children, single parents, seniors, people with special needs and others). A range of housing types will provide flexibility over time.*

It is planned to have 4 semi-detached units in two blocks and 10 townhouse units in two blocks as well. All dwellings will be between 155 to 192 square metres of living space over 2 levels. Generous rear yards and attractive front treatments will enhance the streetscape.

- viii) **Residential Typologies:**

- 1) *Semi-detached: - The guidelines of single and semi-detached residential buildings will provide flexibility in design that encourages a model of development that enhances the look and feel of the community, while integrating with the existing neighbourhoods.*
- 2) *Townhouses: - Townhouses will provide more compact, higher density housing choices than single or semi-detached dwellings and, in some instances, may share outdoor and amenity space. Townhouses may provide the transition between the low density/low-rise housing and more intense multi-residential forms.*

The semi-detached buildings are located to the rear of the development where they are surrounded on three sides by the established and prevalent single detached residences. The eastern portion of the development will accommodate the townhouses and are bordered by a single-family dwelling to the south, a potential multi-unit apartment

building to the north and commercial/institutional establishments across William Street to the east. All units will have their own private domains but share a common access way, services and common element landscaping/lighting.

**The Guidelines relating to building typology policies for building variation and density:**

- i) *A range of housing types within neighbourhoods should be encouraged to promote variety and diversity and address changes in market conditions. Housing types may include detached, semi-detached, townhouse, back-to-back townhouses and/or apartments.*

A mixture of 4 semi-detached and 10 townhouses are proposed.

- ii) *Housing variety should be achieved on each street and block as a means of strengthening neighborhood character and providing more choices. Repetition of design (i.e. style, elevation and materials) should be allowed where repetition of building elements is a characteristic of the building or dwelling type.*

The 4 semi-detached are similar in design with 2 the same and 2 reversed. The townhouses have 3 interior units similar and 3 reversed while the end units have 2 similar and 2 reversed.

- iii) *Higher density development should occur in areas that benefit from increased population and have a variety of movement and travel options, including sites located close to;*
  - 1) *The Mixed Use/Corridor Area;*
  - 2) *Large public open spaces; and*
  - 3) *Larger institutional/community uses.*

The townhouses are located closer to the Arterial Road of William Street and will benefit from the close proximity of the services provided along this route.

- iv) *High density development should transition to adjacent low-rise residential areas through appropriate setbacks and building forms.*

The development provides for a transition from the proposed high density to the north to the low density to the south. Larger than required rear yards and large distances between the front face of the buildings to the south property line will add to a milder transition.

**The Guidelines relating to building typology policies for building height:**

- i) *The following table generally summarizes the range of appropriate heights for typical housing types: Semi-detached 2-3 storeys; Townhouse (Row) – 2-4 storeys.*

The development proposes that all buildings are to be limited to 1 ½ storey structures.



- ii) *Height transition should be incorporated into the design of taller buildings, especially when located adjacent to low density, low-rise areas.*

Height transition is not an issue with this development.

**The Guidelines relating to building typology policies for residential orientation:**

- i) *The main dwelling façade should be located parallel to the street and/or sidewalk, open space or park and in general, line up with adjacent buildings to frame the street. Where the front entrance is accessed from the side yard, the main dwelling façade may be located perpendicular to the street provided that the dwelling façade fronting the street has attractive architectural design and fenestration.*

The development assumes that the units are freehold entities with their front yards facing the private lane and not William Street. All townhouses have entrances from the front face. The semi-detached have side entrances but have front façades that are enhanced similar to the townhomes.

- ii) *Dwellings on corner and flanking lots should be designed so both exposed façades are oriented towards the street. At these locations, building elements and design should emphasize their visibility and potential role as landmarks or orienting structures within the community.*

The sides of the townhomes are treated with architectural relief and fenestration to add interest to the structure visible from William Street.

**The Guidelines relating to building typology policies for residential setbacks:**

**Front Yard**

- i) *All residential front yards should have a minimum of 1.5 metre “no encroachment” area. The balance of the setback may contain non-interior building elements including porches, steps, roof elements, etc.*

There are no encroachments into the front yards of the units.

- ii) *A slight articulation of the front yard setbacks along any street is recommended in order to achieve a diversity of setbacks to the streetscape. Front yard setbacks should generally be a minimum of 3.0 metres to allow for the provision of a useable front porch and allow for a transition between the public and private realm.*

There is a minimum of 6.3 metre front yard setback for the townhouses which exceeds the minimum standard of 6.0 metres in R4-21. The semi-detached have a setback of 4.57 which is less than 6.0 metres but allowed under Committee of Adjustment ruling.

**Rear Yard**

- i) *On lots accessed by a driveway, the minimum rear yard setback should be 7.5 metres measured from the rear face of the garage or rear property line to the rear of the dwelling.*

The minimum setback of the townhouses is 11.44 meters whereas the by-law requires 7 metres. The semi-detached have a setback of 6.07 metres from 7.0 metres as allowed for under a Committee of Adjustments ruling due to the irregularity of the rear yard lot lines.

- ii) *Rear yard decks/porches and garden sheds should be permitted as rear yard encroachments, provided the rear yard is a minimum 7.5 metres in length excluding rear yard garages that are attached to the dwelling or are located at the rear of the property (lane or driveway access).*

There are no rear yard garages and the encroachments can be accommodated in the detailed design.

**The Guidelines relating to building typology policies for articulation and detailing:****Walls**

- i) *The front façade of dwellings and garage treatments should maximize the presence of the habitable building façade through useable front porches, grade level windows including front door windows and sidelights and rooms and/or balconies built above the garage. A high standard of design, detail and variety of materials should be combined to create front building façades with a distinct street presence.*

Except for the semi-detached, the buildings all have front facing covered porches. Large windows, projecting habitable rooms and low-level entrances provide enhance the presence.

- ii) *Flanking façades should have a design and materials standard equal to the front façade treatment.*

The flanking façades have treatment equal to the front.

- iii) *Facing materials, including brick, stone and wood/metal siding, are preferred. A variety of details should be used to break up the façade.*

These materials all have been incorporated into the present design.

- iv) *Wall material should be selected based on energy and maintenance efficiency.*

The design offers energy efficiency in accordance with the latest requirements of the Ontario Building Code yet provides for a robust and resilient exterior finish commensurate with the location of usage.

- v) *Similar (not identical) details and architectural elements should be used to reinforce the continuity of the street and assist in the creation of a strong neighbourhood image by making the buildings relate to each other without being identical.*



Harmonious front façades are proposed with variations to individualize the units. However, the small scale of this development limits the extent due to the lack of economies of scale.

### **Porches and Building Projections**

- i) *Building projections including porches, decks, canopies and stairs are encouraged as transitional building elements that provide weather protection, dwelling access and active amenity spaces.*

Small covered entrance porches have been incorporated in all designs. Stairs will be minimized to facilitate barrier free possibilities.

- ii) *The design of porch railings and columns should be integrated and use complementary materials.*

Decorative columns have been specified for the porch roofs.

- iii) *Finished materials should extend to all sides of the porch and stairs. The underside of the porch should not be exposed to the street.*

The porches will be concrete slab-on-grade or a structural slab above a cold cellar. It is not intended to have any crawl space below the porch slab.

- iv) *Continuity of front porch design is recommended between townhouses, attached and semi-detached dwellings. Material and detail variations may occur between porches provided the scale and proportion is maintained.*

All porches will be of the same material.

- v) *Wrap around porches/verandas are encouraged on corner lots dwellings or other locations where the side yard of the dwelling is visible.*

Due to the pairing of the units, wrap around porches are not possible.

- vi) *For residential units on the ground floor with direct access from the street, privacy should be enhanced through the creation of a private and/or semi-private outdoor amenity space (including lawns).*

The rear yards provide ample outdoor amenity. Fencing may be possible should delineation of property lines become necessary.

### **Windows**

- i) *Buildings facing or flanking a street, lane or open space should provide a generous amount of window openings to encourage strong visual connections between the private dwelling and public realm.*

Large windows facing the public realm as well as private areas are specified in all units.

- ii) *Window design should be primarily an expression of the interior dwelling use. Creative arrangements of windows should have a functional role in providing*

*natural ventilation and light, views and privacy to the individual and adjacent dwellings.*

Window treatment includes soldier coursing of the brick veneer, curved arches and windowed garage doors.

- iii) *Skylights and clerestory windows are encouraged. Skylights should be treated as distinct roof elements and be coordinated with other roof and building elements. Skylights are encouraged to be located behind the roof ridge away from the street view. Clerestory windows should be detailed to provide a structural and coordinated junction between the building wall and roof.*

There are no skylights or clerestory windows proposed. Upper floor areas are to receive natural light and ventilation through dormer windows.

### **Roofs**

- i) *A variety of roof shapes should occur on each block to create individuality of address through differing roof forms. This variety is not required where similar rooflines are a characteristic of the building or dwelling type (i.e. townhouses and semi-detached dwellings).*

The semi-detached exhibit a multi-faceted roof profile whereas the roofs of the townhomes are broken up by dormers and gable end roof projections.

- ii) *Roof forms should apply a generally consistent roofline in mass and height to adjacent buildings.*

This development proposes 1 ½ storey dwellings which will fit well with the adjacent buildings to the south and west that are single and two storey residences.

- iii) *Roof materials/colours should complement the building materials and the proposed building design.*

To be determined and specified during the final design and approval.

- iv) *Where sloped roofs are required, a minimum 30-degree slope is recommended.*

The majority of the roof lines exceed the recommended minimum 30-degree slope. The exceptions are the rear dormers of the semi-detached and the porch roofs of the townhouses which are set at 22.6 degree.

- v) *Townhouse and multiplex dwellings should express individuality of address through defined roof forms that express individual dwellings and contribute to a residential character for the overall development.*

The roof lines are broken by dormers and multi-faceted roof pitches and gable ends.

- vi) *Roof elements including chimneys, dormers, pitches, cupolas and vents should be incorporated as distinct elements providing the potential for additional variety in the image of one dwelling to the next.*



Dormers and roof pitches are varied and additional distinct elements will be dealt with during the final design.

*vii) Incorporating false windows and dormers into buildings is discouraged.*

False windows are incorporated over the joined garages of the semi-detached and the projected habitable rooms of the townhomes to provide a visual relief in the vertical wall of the gable end.

**The Guidelines relating to building typology policies for attached garages:**

- i) Garages should be designed so that they are not the dominant feature in the streetscape. Garage door widths should be minimized and should not be wider than 50 percent of the house width. Options to reduce the impact of the garage include setting back the garage face from the principal façade, building a second storey above the garage, integrating glazing and other architectural details within the garage face.*

The widths are less than 50% of the unit widths and have upper glazing incorporated into their fabrication.

- ii) Attached garages should not project beyond the front façade of the dwelling or the façade of a porch.*

The garages do not project beyond the front exterior walls of the habitable rooms.

- iii) Tandem garages (one car parked behind another) are encouraged where the house and the lot depth permit, as a method of reducing garage frontage, decreasing the width of curb cuts, increasing the living area located at the front of the dwelling and increasing landscaping opportunities in the front yard.*

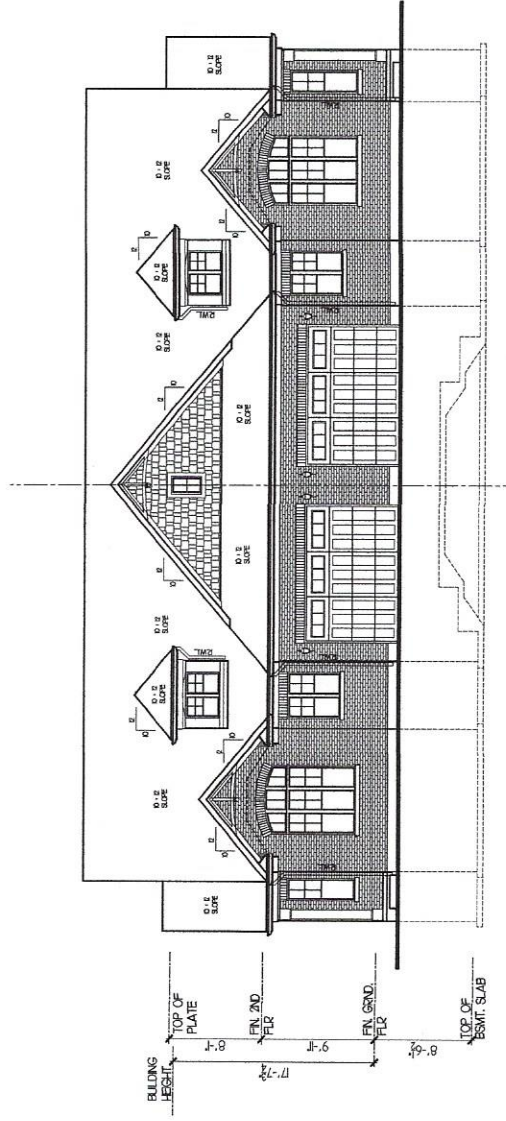
The garages of the adjoining units are paired and lessen the number of curb cuts, albeit not the overall length. This allows for more unified landscaping among the front yards of the lots. The setback of the units also facilitates space for an additional vehicle between the garages and the street line.

- iv) Garage design should be complementary in character and the quality of detail to the principal dwelling, including construction materials, adequate windows and appropriate architectural details.*

The garages are specified to be architecturally panelized with upper window lights.

- v) Rear yard garages accessed by laneway or front driveway are encouraged particularly where homes front on arterial roads, parks and schools to promote greater variety and flexibility in the design of the front façade and front yard.*

There are no rear yard garages.



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MODEL M44-1882

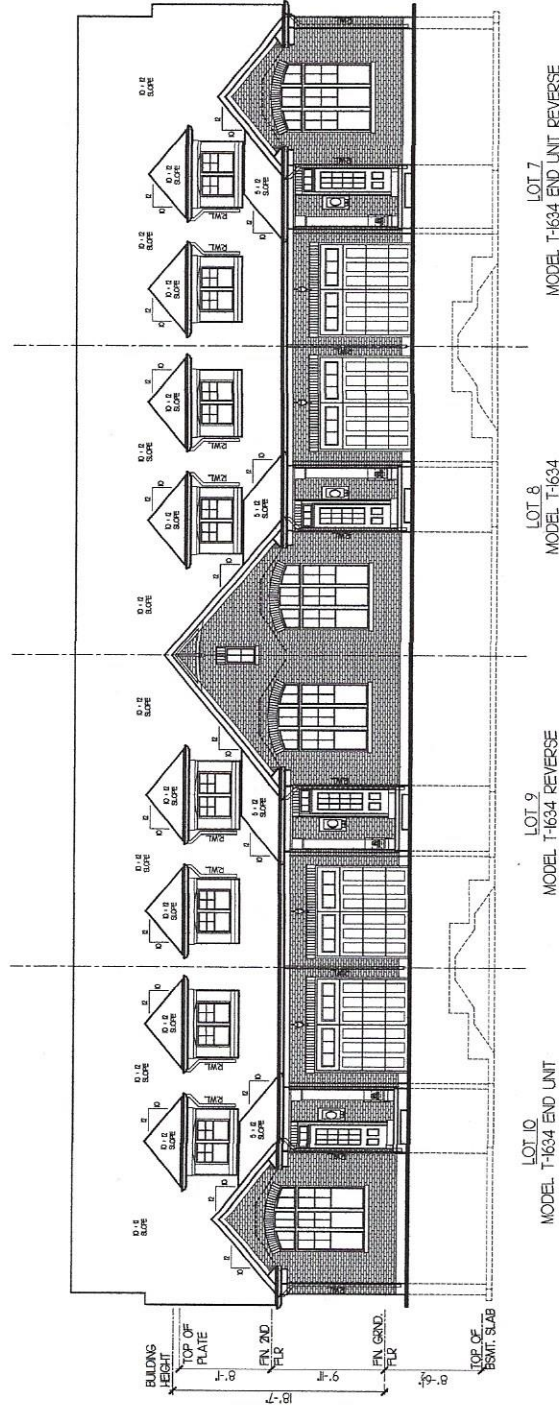
LOT 11  
MODEL M44-1882 REVERSE

FRONT ELEVATION - LOTS 11 & 12

No.	DESCRIPTION	DATE	BY
1	ISSUED FOR PRELIM. PRICING & TRUSS COORD.	APR. 25 / 19	DMK
2	ISSUED FOR STRUCTURAL ENGINEERING	AUG. 8 / 19	DMK
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<p>Client: JOSHANI HOMES</p> <p>Project: WILLIAM ST. TOWN-HOUSES &amp; SEMI-DETACHED 377 WILLIAM ST., COBOLURG, ONTARIO.</p> <p>Scale: 3/32"=1'-0"</p> <p>Date: Aug. 2019</p> <p>Drawn By: DMK</p> <p>Checked By: DMK</p> <p>Sheet Title: FRONT ELEVATION - LOTS 11 &amp; 12</p>		<p>Project Number: IB-009</p> <p>Drawing Number: 5 OF 7</p>
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FRONT ELEVATION - LOTS 7 TO 10

No.	DESCRIPTION	DATE	BY
1	ISSUED FOR PRELIM. PRICING & TRUSS COORD.	APR. 2 / 19	DMK
2	ISSUED FOR STRUCTURAL ENGINEERING	AUG. 6 / 19	DMK
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<p>Client: JOSHANI HOMES</p> <p>Project: WILLIAM ST. TOWNHOUSES &amp; SEMI-DETACHED 377 WILLIAM ST., COBOLURG, ONTARIO.</p> <p>Sheet Title: FRONT ELEVATION - LOTS 7 TO 10</p>		<p>Scale: 3/32"=1'-0"</p> <p>Date: Aug. 2019</p> <p>Drawn By: DMK</p> <p>Checked By: DMK</p>	<p>Project Number: 18-009</p> <p>Drawing Number: 5 OF 7</p>
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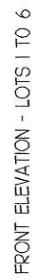
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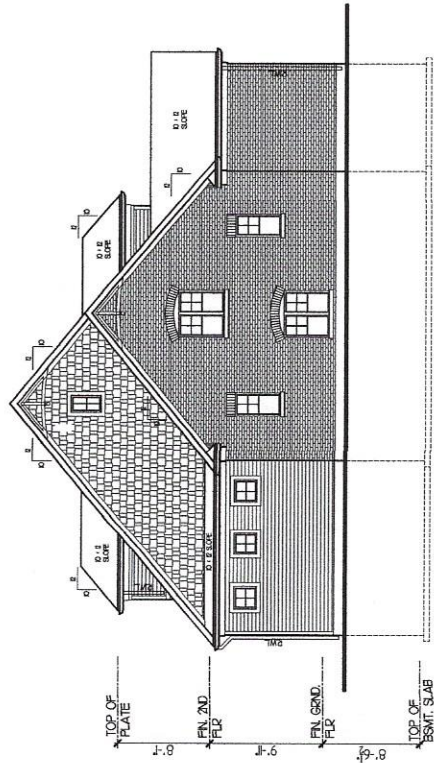
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Client	JOSHANI HOMES	Scale	3/32"=1'-0"
Project	WILLIAM ST. TOWNHOUSES & SEMI-DETACHED 377 WILLIAM ST., COBOURG, ONTARIO.	Date	Aug. 2019
		Drawn By	DNK
Sheet Title	FRONT ELEVATION - LOTS 1 TO 6	Checked By	DNK

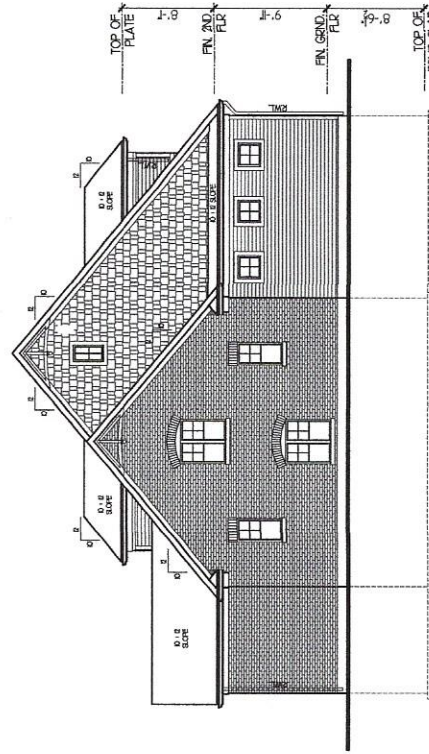
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LOT 6  
MODEL T-1634 END UNIT

LEFT ELEVATION



LOT 1  
MODEL T-1634 END UNIT REVERSE

RIGHT ELEVATION

No.	DESCRIPTION	DATE	BY
1	ISSUED FOR PRELIM. PRICING & TILES COORD.	APR. 25 /19	DMK
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Client <b>JOSHANI HOMES</b>		Scale <b>3/32"=1'-0"</b>	Project Number <b>18-009</b>
Project <b>WILLIAM ST. TOWNHOUSES &amp; SEMI-DETACHED 377 WILLIAM ST., COBOURG, ONTARIO.</b>		Date <b>Aug. 2019</b>	Drawing Number <b>7 OF 7</b>
Sheet Title <b>LEFT &amp; RIGHT EL., - LOTS 1 TO 6</b>		Drawn By <b>DMK</b>	
		Checked By <b>DMK</b>	

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## 4.0 Planning Opinion and Conclusion

The applicant, Joshani Homes Ltd. proposes to develop the site at 377 William Street into a freehold condominium complex consisting of 4 semi-detached and of 10 townhouse units in 2 blocks. All units will front onto a private common element lane which will be owned, operated and maintained by the owners of the units and have direct access to William Street. The services such as storm sewer, fire protection, sanitary sewer, garbage collection, snow removal, lane lighting will be borne by the owners of the units on a proportional basis. Individual services such as gas, hydro, water, communications and maintenance of individual lots will be the responsibility of the individual owners.

We have reviewed the proposal with due regard to the requirements and recommendations of the Town of Cobourg Official Plan and the Urban and Landscape Design Guidelines. We have modified the plan to reflect these documents and submit them for staff comments. It is our opinion that the proposal meets or exceeds these requirements and recommendations and represents an economical and sustainable development with due regard to the natural and surrounding environment, accessibility and serviceability.

**R.W. BRUYNSON INC.**



**R. W. Bruynson, P.Eng., OAA  
Architect/Consulting Engineer**

